

**CITY OF LOS ANGELES**  
CALIFORNIA



ANTONIO R. VILLARAIGOSA  
MAYOR

DEPARTMENT OF  
**CITY PLANNING**  
200 N. SPRING STREET, ROOM 525  
LOS ANGELES, CA 90012-4801  
AND  
6262 VAN NUYS BLVD., SUITE 351  
VAN NUYS, CA 91401  
C  
CITY PLANNING COMMISSION  
WILLIAM ROSCHEN  
PRESIDENT  
REGINA M. FREER  
VICE-PRESIDENT  
SEAN O. BURTON  
DIEGO CARDOSO  
GEORGE HOVAGUIMIAN  
JUSTIN KIM  
ROBERT LESSIN  
DANA M. PERLMAN  
BARBARA ROMERO  
JAMES WILLIAMS  
COMMISSION EXECUTIVE ASSISTANT II  
(213) 978-1300

**EXECUTIVE OFFICES**

MICHAEL J. LOGRANDE  
DIRECTOR  
(213) 978-1271  
ALAN BELL, AICP  
DEPUTY DIRECTOR  
(213) 978-1272  
LISA M. WEBBER, AICP  
DEPUTY DIRECTOR  
(213) 978-1274  
EVA YUAN-MCDANIEL  
DEPUTY DIRECTOR  
(213) 978-1273  
FAX: (213) 978-1275

INFORMATION  
[www.planning.lacity.org](http://www.planning.lacity.org)

August 17, 2012

Honorable Members of the City Council  
Transportation Committee & Arts, Parks, Health & Aging Committee  
c/o Office of City Clerk  
Room 395, City Hall  
Mail Stop 160

Attention: Adam Lid, Legislative Assistant  
John White, Legislative Assistant

Honorable Members:

**Report on "Parklets" Council File No. 11-1604 (Perry / Huizar, Reyes)**

On September 16, 2011, Council members Huizar and Perry introduced a motion (11-1604) requesting that the Planning Department, in consultation with the Departments of Transportation and Public Works, "assist with the implementation of parklet demonstration projects" and report back to the Transportation Committee and Arts, Parks, Health & Aging Committee on the creation of a citywide parklet program similar to San Francisco's 'Pavement to Parks' program."

**Background**

A parklet is a small urban pedestrian space, often created by replacing several under-utilized parallel or parking spots with a patio, planters, trees, benches, café tables with chairs, fountain(s), artwork, sculptures and/or bicycle parking. Parklets expand the width of sidewalks for a variety of uses that encourage pedestrian and multi-modal activity. Parklets are designed to provide a public place for citizens to relax and enjoy the atmosphere of the city around them, in places where either current urban parks are lacking or if the existing sidewalk width is not large enough to accommodate vibrant street life activities. Parklets can be designed to be either semi-permanent fixtures, or temporary/seasonal in places.

## **Inter-Department Coordination**

The Departments of Public Works, Transportation and City Planning have worked closely on creating a pilot program. After analyzing the issue, participating in a series of meetings organized by Council Districts 9 and 14, coordinating field trips to Long Beach, and researching existing "parklet" and similar programs, staff from the Planning Department's Urban Design Studio and Department of Transportation and Public Works recommend a proposed set of guidelines (Attachment A) for permitting the four pilot demonstration projects (Attachment D) and recommend using a model similar to San Francisco's for establish a RFP program in Los Angeles (Attachment B).

## **Other Jurisdictions**

In recent years, municipalities have experimented with new models to improve temporary and semi-permanent open space by reclaiming portions of the public right-of-way for park and recreation purposes. In 2009, the City of New York pedestrianized Broadway in Times Square, adding moveable seating and sidewalk paint to create open space in the heart of the city. In Los Angeles, Streets 4 People, working with the Department of Transportation and the Planning Department's Urban Design Studio, has created the Sunset Triangle Plaza out of excess roadway. The creation of a parklet program is an extension of this concept. Such programs are cost-effective public improvement projects coordinated inter-departmentally through City agencies, elected officials and community partners.

In 2010, San Francisco created the "Pavement to Parks" program to facilitate the creation of "parklets" throughout the city, offering a new approach to achieve a similar outcome. The Pavement to Parks model allows private parties (BIDs, community groups, business owners, etc.) to propose installing a parklet. The permits are revocable and, therefore, the structures temporary. The applicant must pay the permitting costs as well as bear the costs for the design, installation, maintenance of, and liability for the parklet. If the parklet is approved in the request for proposals, the City contributes only the loss of income from the on-street parking spaces but gains pedestrian and bicycle amenities. The Pavement to Parks model provides a simple, low cost, and effective approach to the creation of new urban space and pedestrian amenities without requiring significant investment in new infrastructure. This can be done in a relatively short period of time. Several cities across the United States and Australia have permitted or begun developing a parklet program. In California, the city of Long Beach has permitted pilot parklet projects and Oakland has created a parklet permit program. A series of photographs from San Francisco, Long Beach, Oakland, and Chicago are included in the report (Attachment C).

## **Potential Benefits of a Parklet Program**

By December of 2011, San Francisco had 23 parklet projects installed throughout their city and more are under construction. They have issued two subsequent requests for proposals (RFPs). Following the close-out of a third RFP period in December 2011, several more parklet projects are expected within the next few months. Most parklet projects have been installed in front of and near private businesses, serving a dual

purpose of adding outdoor seating space as well as accessible public space. Media outlets including the New York Times, Chicago Tribune, and San Francisco Chronicle have written on the benefits of this innovative model of incentivizing private investment in the creation of the public open space. The Pavement to Parks model allows business improvement districts, community organizations, or even individuals to apply to the parklet RFPs, as well as business owners.

### **Creating a Program for Los Angeles**

A program similar to "Pavement to Parks" in Los Angeles will have the potential to increase sidewalk activity, provide public amenities, and economic development opportunities throughout the City. It will provide a low cost alternative for building community amenities.

Following the Pavement to Parks model, a parklet program could be permitted in the City of Los Angeles by creating a new type of revocable permit. As revocable permits, a parklet would be temporary in nature, be designed on support structures similar to those used for creating decking on roofs, provide for the passage underneath of water and debris into the storm water system, and be modular in design to allow for access to clean underneath and for quick removal if required. Staff would like to first permit the four pilot parklet projects and evaluate the design criteria, resources, and requirements for establishing a program in Los Angeles.

### **First Step is Pilot Parklet Projects**

Council District 14 has initiated the creation of two parklet "demonstration projects" on North Huntington Drive and York Boulevard while the Downtown Los Angeles Neighborhood Council (DLANC) has established an ad-hoc working group comprised of Downtown residents skilled in architecture, industrial fabrication, social media, planning, urban design, and other trades, to design, build, seek funding, and install "parklet" demonstration projects on Spring Street in coordination with local business owners, UCLA Luskin School of Public Affairs, Lewis Center for Regional Policy Studies, and Institute of Transportation Studies, and the Historic Downtown Los Angeles Neighborhood Business Improvement District (HDBID).

The four proposed demonstration pilot parklet projects provide an opportunity for the City to evaluate the benefits of such projects, provide a chance to refine the guideline criteria, allow for the construction of the parklet through the necessary channels in the Departments of Transportation and Public Works, and to allow the creation of a parklet program based on the pilot projects. The guidelines in (Attachment A) present an approach to approve parklet projects based upon community support, safety and accessibility features. For example, a parklet must be open to the public, be ADA compliant, and not take up the full dimensions of the parking spaces, so that cars and bicyclist may safely pass. The community must be in support of the project. The pilot projects will be built and maintained by local community stakeholders. The projects will be required to provide maintenance agreement, waiver of damages, insurance similar to the requirements for the pilot Street 4 People Sunset Triangle Plaza project.

Further, Attachment B contains the documentation from San Francisco establishing the Pavement to Parks Program. If the City of Los Angeles were to follow such a model, the pilot parklet program will test the procedures for uses in the public right-of-way, help formulate the criteria for creating a new permit category, and work out the issues for creating a program in the City of Los Angeles.

**Recommendations for Council Action:**

Staff suggests the following for your consideration and recommendation to the City Council:

1. Adopt this report regarding pilot demonstration projects, parklet design parameters, and program recommendations.
2. Approve the construction of the four pilot parklet projects in the public right of way and direct the Department of Public Works to permit the pilot projects at the four sites in accordance with the plans reviewed by the Departments of Transportation, Public Works and Planning (Attachment D).
3. Direct the Department of Transportation to negotiate and execute a memorandum of understanding with the community stakeholders setting forth the terms and conditions of the pilot demonstration parklet projects.
4. Direct the Departments of City Planning, Transportation, Public Works and any other relevant department to prepare a report within six months of the completion of the pilot parklet projects containing recommendations for creating a Parklet Program. It is to include lessons learned from the pilot projects, recommendations for the creation of a permit process, draft ordinance language, cost analysis, and recommendation for the responsible city department to house the program.

If you have any questions, please contact Simon Pastucha, Head of the Urban Design Studio, Plan Implementation Section at (213) 978-1475.

Sincerely,

  
Michael J. LoGrande  
Director of Planning

## ATTACHMENT A

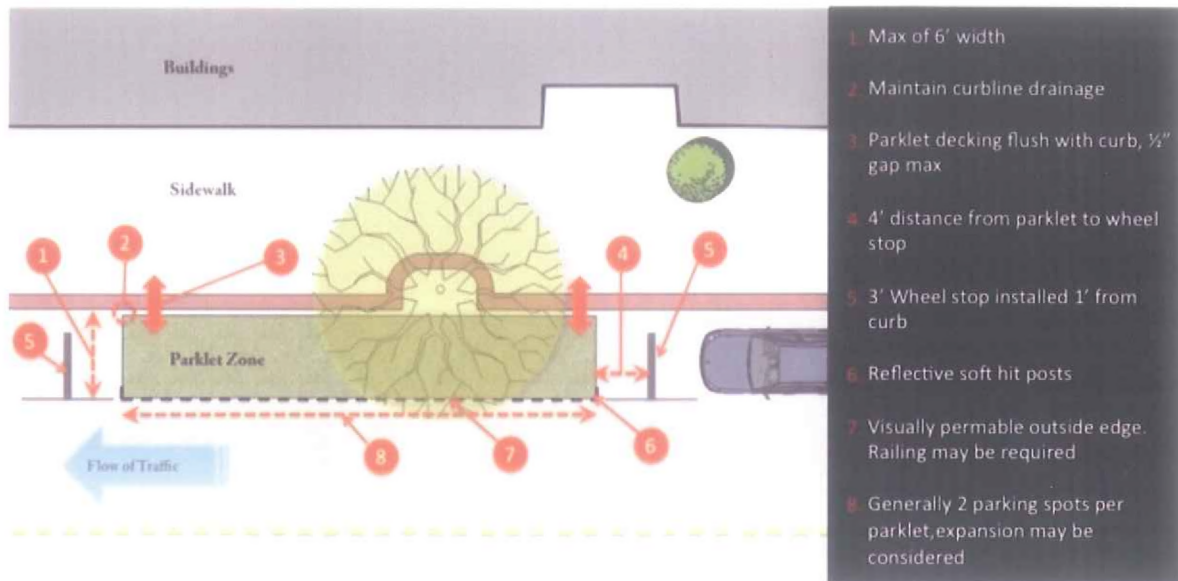
### Proposed Parklet Design Parameters for Approval of Demonstration Projects

#### 1. Location & Requirements:

- Parklet must be located away from a corner (at least one parking spot in from corner) and along a street with a speed limit of 25mph or less. A 15-foot buffer between the edge of the closest travel lane and the parklet must be provided on streets with a speed limit of 35 MPH or more with Los Angeles Department of Transportation (LADOT) approval.
- Parklet must not extend beyond six feet from the curb line in places where there is parallel parking.
- Safe hit posts and two 3' parking wheel stops are required (must be installed within one week or less upon receiving permit).
  - *Refer to San Francisco Guideline 4 in Permit\_Recipient\_Guide\_FINAL.pdf*
  - Street-facing materials used should be able to absorb impact of errant vehicle to prevent penetration of parklet but not to cause severe injuries to occupants of vehicle.
  - The parking wheel stop may be required to have a minimum of 4-foot clearance from nearest on-street parking space.
  - *Refer to San Francisco's Guideline 6 in Permit\_Recipient\_Guide\_FINAL.pdf*
- Applicant is required to provide an edge to Parklet (depending on location).
  - Edge should be visually permeable.
  - When cable railing is used, vertical spacing between cables cannot exceed 6".
- Parklets are not permitted in red zones unless approved by LADOT.
- Parklets can replace yellow, blue, or motorcycle parking zones if applicant pays relocation fees.
- Parklets are allowed in white and green zones if the original entity agrees to repurpose curb.
- Parklets should meet ADA standards for accessibility.
- Parklets are not permitted in front of a fire hydrant or in a way that restricts access to any public utility company or City utility covers, valves, etc.
- Parklets are required to include access panels for the purpose of maintaining the gutter and area underneath the Parklet and be design to allow drainage along the gutter to pass underneath the Parklet.
  - The gutter must be kept free of debris at all times
- Parklets may not use tropical hardwood or virgin redwood.
- Applicant must have two "Public Parking" signs fabricated.
- Design Parameters Visual:
  - *Refer to " San Francisco's Design and Placement Guidelines" in Parklet\_Call\_for\_Projects\_110711.pdf (p.7)*

#### 2. Furniture:

- Applicant is required to include some sort of permanent furniture on your Parklet, such as, but not limited to, benching or bike racks.
- Furniture used on the Parklet must be different from any existing sidewalk furniture and table service is not permitted.
- All seating must be publicly accessible at all times. A standard sign posted on the Parklet will stipulate this.
- Any movable items, such as tables, planters, and chairs, must either be locked down at night or taken inside. Unsecured furniture is not permitted after business hours.



Draft Recommendations. Subject to further refinement.

## **ATTACHMENT B**

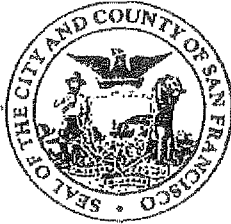
### **Documents establishing San Francisco's Pavement to Parks Program**

*San Francisco's Parklet Approval Directive Establishing Guidelines*  
*San Francisco's "Design and Placement Guidelines" in Parklet Call for Projects 110711*  
*San Francisco's Permit Recipient Guide*  
*San Francisco's Parklet Maintenance Application*

**ATTACHMENT B**

Documents establishing San Francisco's Pavement to Parks Program

*San Francisco's Parklet Approval  
Directive Establishing Guidelines*



Gavin Newsom, Mayor  
Edward D. Reiskin, Director



(415) 554-5810  
FAX (415) 554-5843  
<http://sfdpw.org>

Department of Public Works  
BUREAU OF STREET-USE & MAPPING

875 Stevenson Street, Room 460, S.F., CA 94103

Jerry Sanguinetti, Bureau Manager

**DPW Order No: 178,939**

ESTABLISHING GUIDELINES FOR THE APPROVAL AND INSTALLATION OF TEMPORARY SIDEWALK EXTENSIONS (PARKLETS) FOR USE BY THE GENERAL PUBLIC AT APPROPRIATE LOCATIONS WITHIN PUBLIC RIGHTS-OF-WAY.

- I. PURPOSE: Public Works Code Article 16, Section 810 governs the installation of sidewalk landscaping. This Department of Public Works (DPW) Order provides detailed implementation guidelines for the approval and installation of temporary sidewalk extensions (Parklets) consistent with the sidewalk landscaping program.
- II. BACKGROUND: Parklets provide an economical solution to the desire and need for wider sidewalks and are intended to provide space for the general public to sit and enjoy the space where existing narrow sidewalks would preclude such occupancy. Parklets are intended as sidewalk/street furniture, providing aesthetic elements to the overall streetscape.
- III. APPLICATION AND REVIEW:
  - A. The following applicants are eligible to apply for the installation of Parklets within the public right-of-way:
    1. Community Benefit Districts (CBDs)
    2. Ground floor business owners
    3. Non-profit and community organizations
    4. Fronting property owners
    5. Other applicants may be considered on a case by case basis.
  - B. The following shall be included in the application:
    1. A letter requesting the Parklet and an initial site plan showing the footprint/outline of the proposed Parklet, including approximate dimension of Parklet, property lines, existing sidewalk width, existing parking stalls/alignment and all existing sidewalk furniture and obstructions; e.g. fire hydrants, utility poles, parking meters, street trees, etc.
    2. Type(s) of elements being proposed to be placed on the Parklet; e.g. Tables & Chairs, benches, planters/landscaping, bicycle parking, etc.
    3. A description of how the proposed Parklet meets each of the criteria set forth in Section V of this DPW Order.
    4. Provide documentation of support from adjacent property/business owners. Documentation of support from any existing merchant or neighborhood associations is strongly encouraged.
    5. An application fee of \$215, payable to Department of Public Works (DPW).
  - C. Each application shall be reviewed by a inter-agency review team, with representation from DPW, MTA, City Planning, et al, as necessary, specifically convened to review Parklet applications with each proposal reviewed based on the following criteria:
    1. Meets established design criteria.
    2. Enhancement of streetscape quality and preliminary design.
    3. Location (Parklet is likely to be well used and active).
    4. Community support.
    5. Maintenance plan.
  - D. If a recommendation is made to approve the proposed Parklet:

1. DPW will issue a Notice of Intent to Approve Parklet. The applicant shall be required to post this Notice in a readily visible location in front of the property where the Parklet will be located for ten (10) calendar days from the date listed on the Notice.
  2. If there are no objections from the public, the applicant shall be required to submit the following information for further review:
    - a. Final dimensioned site plan, including all details, finishes, plant species, furniture types, etc.
    - b. Maintenance details, including access panels and how drainage will be provided along the existing gutter.
  3. If there are objections from the public, DPW shall schedule a public hearing to consider the proposed Parklet.
  4. The DPW Hearing Officer shall consider and hear all testimony in support and in opposition to the proposed Parklet and make a recommendation to the DPW Director.
  5. The DPW Director, in his or her discretion, may recommend approval or conditional approval of the permit subject to further review and final action.
  6. If the DPW Director recommends approval or conditional approval the permit, see #2a and #2b above for additional submittal requirements.
- E. If the application is disapproved, DPW shall notify the applicant, upon which the applicant may appeal the disapproval of the permit by the DPW Director to the Board of Appeals within fifteen (15) days of the Director's decision.


#### IV. APPROVAL PROCESS:

1. The inter-agency review team (See Section III. Paragraph C.) shall review the final detailed site plan and maintenance details.
2. Once the review team makes a recommendation for DPW to approve the final plan and the permit, the applicant shall submit the following information and fees to DPW for permit issuance:
  - a. A Certificate of Insurance naming the City and County of San Francisco as additional insured, with general liability coverage of not less than \$1 million.
  - b. An additional permit fee pursuant to Section 2.1.3 of the Public Works Code. While each proposal will result in different additional permit costs based on the time and materials costs incurred by the City in review of the proposal, DPW anticipates an average cost of \$400.
3. Any interested person may appeal the approval of the permit decision by the DPW Director to the Board of Appeals within fifteen (15) days of the Director's decision.
4. The permit shall be renewed annually. Prior to expiration of the annual permit term, the Permittee shall submit to DPW a current Certificate of Insurance and a permit renewal fee of \$215.


#### V. APPROPRIATE LOCATION AND DESIGN PARAMETERS:

- A. The proposed Parklet site shall be located at least one parking spot in from a corner or protected by a bollard, sidewalk bulb-out, or other similar feature, if located at the corner.
- B. The proposed location shall have a posted speed limit of 25 mph or less. In the event the posted speed limit is changed, the Parklet permit is subject to revocation. Streets with higher speed limits may be considered on a case by case basis.
- C. The proposed street has parking lanes that will not become a tow away lane during morning or afternoon peak hours, and the Parklet may not extend beyond six (6) feet from the curb line where there is parallel parking or fifteen (15) feet from the curb line where there is diagonal parking.
- D. The proposed street has a street grade of no greater than 5%.
- E. Parklets shall be required to have reflective tape, soft hit posts, wheel stops, and depending on the proposed location, may also require edging such as planters, railing or cables.
  - a. Any edge shall be visually permeable.
  - b. If cables are used, vertical spacing between cables may not exceed 6".

- F. Parklets shall not be allowed in red and blue zones.
- a. Parklets may replace yellow zones or motorcycle parking if there are appropriate adjacent locations for these zones to be relocated, and the applicant is willing to pay additional fees for relocating these zones.
  - b. Parklets may be allowed in white and green zones if the business that originally requested the white and/or green zones agrees to re-purpose that curb area for use as a Parklet.
- G. Parklets shall not be allowed in front of a fire hydrant, or over a manhole or public utility valve or cover.
- H. All elements of the above mentioned Parklets shall be constructed and/or installed to conform to the applicable provisions, rules, regulations and guidelines of San Francisco Building Code (SFBC), The Americans with Disabilities Act (ADA) and the Americans with Disabilities Act Accessibility Guidelines (ADAAG).

 [Click here to sign this section](#)

10/18/2010

 Fuad Sweiss, City Engineer

**Signed by Fuad Sweiss**    [View details](#)  
on Monday, October 18, 2010 8:21 AM (Pacific Daylight Time)

10/19/2010

 Edward D. Reiskin

**Signed by Reiskin, Ed**    [View details](#)  
on Tuesday, October 19, 2010 8:59 AM (Pacific Daylight Time)

**ATTACHMENT B**

Documents establishing San Francisco's Pavement to Parks Program

*San Francisco's "Design and Placement  
Guidelines" in Parklet Call for Projects  
110711*



## SAN FRANCISCO PLANNING DEPARTMENT

### REQUEST FOR PROPOSALS FOR TEMPORARY SIDEWALK EXTENSIONS "PARKLETS"

Date RFP Issued: November 7, 2011

Date Initial Responses Due: December 5, 2011, 5:00PM

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
415.558.5378

Fax:  
415.558.5409

Planning  
Information:  
415.558.6377

#### BACKGROUND

Parklets provide an economical solution to the desire for wider sidewalks. Parklets are intended to provide space for people to sit, relax and enjoy the city around them, especially where narrow sidewalks would otherwise preclude such activities. They are intended to be seen as pieces of street furniture, providing aesthetic enhancements to the overall streetscape. Parklets secure this space by repurposing on-street parking spots. In the place of car parking, a platform is built to bring the grade of the sidewalk out into the street. Once the platform is installed, benches, tables, chairs, landscaping, and bike parking can all be placed on top in order to provide a welcoming public space along the street. While Parklets are sponsored by a private partner, all seating on a Parklet must remain free and open for any member of the public to use. Any movable furniture used on the Parklet must be different from any existing sidewalk furniture and table service, including placement of condiments, is not permitted.

#### ELIGIBLE APPLICANTS

1. Community Benefit Districts (CBDs)
2. Storefront business owners\*
3. Non-profit institutions and community organizations
4. Other applicants may be considered on a case by case basis. Please contact [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org) before submitting an application.

#### \*Restaurant use:

If the applicant is a restaurant or cafe that currently has table service, please contact us (email at the end of this RFP) before you submit your application. We will work with you to develop an agreement that details the specifics of the no table service provision.

#### APPROPRIATE LOCATION AND DESIGN PARAMETERS

Please see the attached handout for specific design parameters. Generally, the Parklet must be located away from a corner and along a street with a speed limit of 25mph or less. The width of the Parklet must not extend beyond six feet from the curb line in places where there is parallel parking. In locations where parking is angled or perpendicular, please email [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org) before applying.

Safe hit posts and wheel stops will be required. Please see the attached diagram showing the placement of these materials. Depending on the location, you will also be required to provide an edge to the Parklet, such as planters, railing, or cabling. Any edge should be visually permeable. If cable railing is used, the vertical spacing between cables cannot exceed 6". Parklets are not permitted in red zones. They may replace yellow or blue zones or motorcycle parking if there are appropriate adjacent locations for these zones to be relocated and the applicant is willing to pay additional fees to

relocate these zones. They may be allowed in white and green zones if the entity that originally requested the white or green zone agrees to repurpose that curb area. Lastly, Parklets are not permitted in front of a fire hydrant or in a way that restricts access to any public utility company or City utility covers, valves, etc. If you have any questions about your particular location, please email [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org)

Parklets are intended to be aesthetic improvements to the streetscape. We ask that you design them with this in mind, ensuring that the materials you use are high quality, durable, and beautiful. Greening is an important aspect of this beautification. Access panels in order to maintain the gutter and area underneath the Parklet must be included and the design must allow drainage along the gutter to pass underneath the Parklet. By City and County of San Francisco code, Parklets may not use tropical hardwood or virgin redwood.

Lastly, we strongly encourage benches and small permanent seats. If you choose to use tables and chairs, the furniture you use must be different than the furniture that you may currently use on the sidewalk as part of your Café Tables & Chairs Permit or inside your business, if applicable. You will also be required to include some sort of permanent furniture on your Parklet, such as, but not limited to, benching or bike racks. This will be a condition of your permit. As mentioned above, all seating must be publicly accessible at all times. A standard sign posted on the Parklet will stipulate this.

## REVIEW PROCESS

### 1. INITIAL APPLICATION: MONTH / DAY / YEAR

Submit your **Initial Application** (see below) by the response date listed on the first page of this document.

### 2. PLANNING DEPARTMENT INITIAL REVIEW

The Planning Department will review your initial application to ensure that you meet the program requirements and that you've documented your support (see below). A staff committee from various City agencies will review the initial set of applications and will rank projects based on the following goals:

- a. Enhancing the aesthetic quality of the streetscape and thoughtful preliminary design
- b. Good location – proposed Parklet is likely to be well used and active
- c. Demonstrated community support
- d. Likely to be well-maintained

We anticipate selecting approximately 25 applications within 2 weeks of submission. Those applicants not selected as part of this initial ranking will be informed.

### **3. PUBLIC NOTICE PERIOD (10 days)**

If selected, the City will post a public notice at your location documenting your application for a Parklet permit. The public notice must be posted for 10 calendar days. (Start of notice period within 4 weeks of submission) The purpose of the notice is to alert the public of your application for a Parklet permit and to provide the opportunity for the public to express its support for or opposition to your application. If no objection is registered, skip to step 4. If an objection is registered, see below:

#### **a. OBJECTIONS RAISED DURING PUBLIC NOTICE PERIOD**

If we receive objections from the public for your location, a Public Hearing will be scheduled. Recommended approval or denial of the application is determined at the Public Hearing. In the event of a denial, the applicant may appeal to the Board of Permit Appeals within 15 calendar days of the decision.

If the DPW hearing officer recommends issuance of a permit, proceed to step 4. If the hearing officer agrees with the objector and recommends not issuing a permit, you may appeal that decision to the Board of Permit Appeals.

### **4. FINAL PLANS SUBMITTED AND PLANNING DEPT. APPROVAL (1 month)**

For those locations where no protest was registered, or after public objection is overridden, we will invite you to submit a fully detailed site plan, cross sections, and elevations. With this invitation, we will provide you with guidance as to what level of detail we will require. You will have one month to develop these drawings. Planning staff will work with your designer in a collaborative fashion during this period to ensure that the parklet design is appropriate and that the final drawings produced are sufficient.

### **5. SFMTA & SFPDW APPROVAL (about 2 weeks)**

The Planning Department will forward your final plans to the Municipal Transportation Agency (SFMTA) and the Department of Public Works (SFPDW) for their sign off. You may receive additional comments from these agencies.

### **6. DPW ISSUES PERMIT**

The Department of Public Works will collect payment and issue your permit.

### **7. CONSTRUCT PARKLET**

With your permit in hand, you must then inform the Department of Public Works no less than 10 days before beginning any site work. Once you've completed installing your Parklet, you must notify DPW and email [sfpavementtoparks@sfplanning.org](mailto:sfpavementtoparks@sfplanning.org)

## WHAT MUST BE INCLUDED IN YOUR INITIAL APPLICATION

1. **Application Form.** The application form is included at the end of this RFP. Please fill it out completely.
2. **Initial site plan.** Please provide a plan (see flyer for an example) that shows the footprint of the proposed Parklet, including enough detailed information to understand what is happening on either end of the proposed Parklet. Please show property lines, sidewalk width, Parklet length and width, existing parking stalls, and all surface obstructions within 15 feet of the occupied area (e.g. fire hydrants, streetlights, parking meters, street trees, etc) on the plan. Accurate measurements are required. The number(s) on all parking meters that are to be removed must also be included in all drawings. These numbers are generally posted on the meter facing the street. We encourage as much detail as possible to help us review your application.
2. **Parklet programming.** Please demonstrate what type of elements you are proposing on the Parklet (e.g. tables and chairs, benches, landscaping, bike parking, etc.)
3. **Support.** We encourage you to adequately and robustly outreach to your neighbors, tenants of the subject property, and any existing merchant or neighborhood groups, if applicable. It is important that you involve your community in the development of the proposal. Please provide documentation of your support. Support can take the form of a letter, a petition, an email, etc.

## COSTS

1. Only after being informed that the City will issue you a permit will you be responsible for the following fees which are based on cost recovery for time and materials only:
  - \$791 base fee for all applications; and
  - \$650 for up to two parking meter removals (required only if meters currently exist); and
  - \$191.50 to pay for inspection of your site before and after installation; and
  - Only if your Parklet proposes the use of three parking stalls or more:
    - \$285 Additional base fee for each parking stall used beyond the first two
    - \$325 Additional fee for each additional meter removal beyond the first two (required only if meters currently exist)
2. All yearly renewals will be charged \$221.00

A single check should be addressed and made payable to the San Francisco Department of Public Works. Please do not submit a check until you are invited to do so.

You will also be responsible for all costs associated with designing and installing the Parklet.

## RESPONSIBILITIES OF THE PERMIT HOLDER

If your application is selected, you will be required to enter into a permit with the City and satisfy the following requirements:

1. **Insurance.** You will be required to provide evidence of at least \$1M in liability insurance (the same requirement as sidewalk café tables and chairs), naming the City and County of San Francisco as additional insured.
2. **Maintenance.** You will be required to sign a maintenance agreement to keep all plants in good health and the Parklet free of debris and grime. You must maintain the surface of the Parklet daily and rinse out the area beneath the Parklet at least once a week. The Department of Public Health may require you to provide pest abatement beneath the Parklet platform.
3. **Tables and Chairs.** Any movable items, such as tables and chairs, must either be locked down at night or taken inside. Unsecured furniture is not permitted after business hours.

#### **FINAL NOTE**

The City reserves the right to amend this RFP and all terms contained within it up until the point that a final permit is issued. Applicants may withdraw their application if they do not agree with any of the amended terms.

#### **QUESTIONS**

For any questions regarding this RFP, please email [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org)

#### **SUBMITTING YOUR INITIAL APPLICATION**

In order to gauge interest, we ask that you notify us as soon as possible via email, [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org), if you intend on applying.

Otherwise, all initial applications are due by December 5, 2011, 5:00PM at the address listed below:

Andres Power  
SF Planning Department  
1650 Mission Street, Suite 400  
San Francisco, CA 94103



## SAN FRANCISCO PLANNING DEPARTMENT

---

### APPLICATION FOR REVOCABLE TEMPORARY SIDEWALK EXTENSION (PARKLET) PERMIT

#### APPLICANT INFORMATION

Location: \_\_\_\_\_

Block#: \_\_\_\_\_

Lot# \_\_\_\_\_

Sponsoring Business/Organization Name: \_\_\_\_\_

24/7 Contact Name: \_\_\_\_\_

Mailing Address (if different than above location): \_\_\_\_\_

Cell Phone: \_\_\_\_\_

Fax: \_\_\_\_\_

Email: \_\_\_\_\_

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
415.558.6378

Fax:  
415.558.6409

Planning  
Information:  
415.558.6377

☐ SF BUSINESS CERTIFICATE \_\_\_\_\_ OR:

☐ DRIVER'S LICENSE \_\_\_\_\_

**ATTACHMENT B**

Documents establishing San Francisco's Pavement to Parks Program

*San Francisco's Permit Recipient Guide*

## GUIDE FOR PERMIT RECIPIENTS

After being granted a Parklet permit, follow these instructions in addition to any conditions stipulated in the actual permit from DPW.

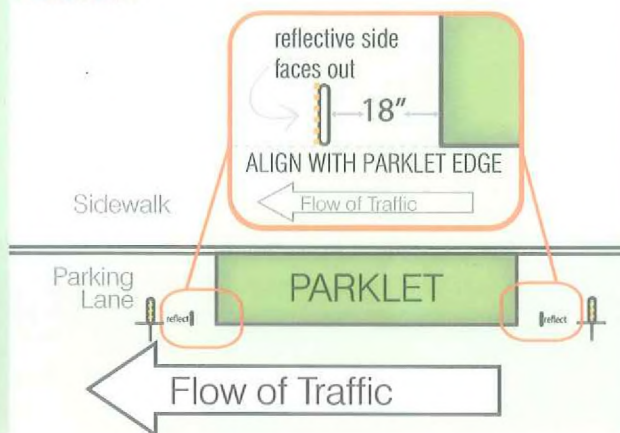
1. Set an installation date and contact us at [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org) to arrange for no-parking signs. Total installation of the Parklet should take no more than 10 days.
2. At least 10 days prior to installation of the Parklet, please contact DPW Division of Inspection Services at (415) 554-7149 to set-up an appointment for a pre-installation site inspection. A follow-up inspection may be required for a final inspection of the installed Parklet.

3. Clear the area for installation by placing no-parking signs in the parking spaces 72 hours before installation.



4. Purchase two standardized safe-hit posts, one for each end of the Parklet.

### PLACEMENT:



### POST SPECIFICATIONS:

- Safe Hit Type 2 Guide Post 36", White
- Surface Mount Pin Lock Base
- Butyl Adhesive Pad or 10 oz Epoxy Kit



You may purchase the posts from any vendor, but they must meet the above specifications. The City purchases its posts at: *Interstate Traffic Control, San Carlos (650) 591-2300*

5. Have two "Public Parklet" signs fabricated. This sign is on file at *LaHue & Associates*. Please let us know if you prefer a different vendor so we can supply you with the template and the standard specifications.

LaHue can be contacted at (415) 206-9136

The signs will be installed by a representative from our department for optimum visibility. **Do not install the signs yourself without permission.**

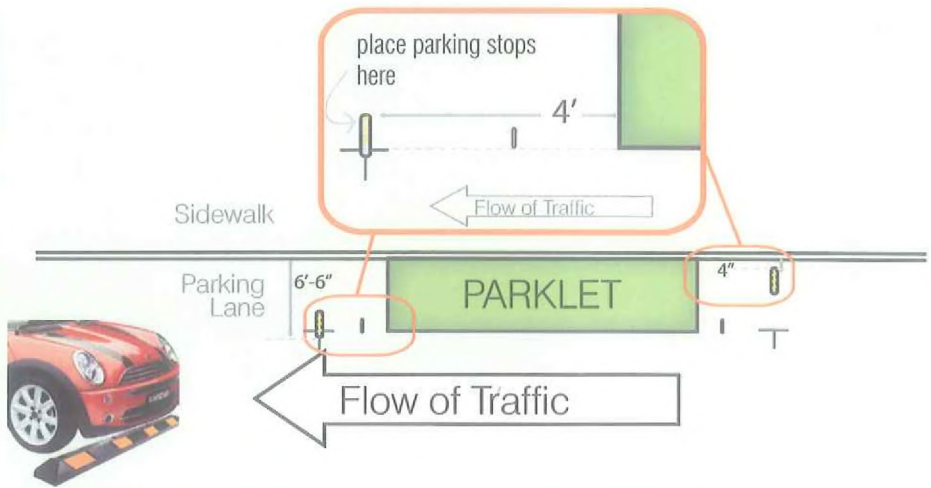


## GUIDE FOR PERMIT RECIPIENTS

## PARKLET PERMIT

6. Install the two 3' parking stops in the exact position stated below unless otherwise instructed:

- Parking stops should be installed 4 feet from outside edge of Parklet, on both sides.
- Looking at the Parklet from the street, outside edge of left side parking stop should be 6'-6" from curb and 4" from the curb on the right side (see image)
- Wheel stop should be 3 feet long, rubber, with yellow stripes and should be mounted with bolts.



You may purchase the stops from any vendor, but they must meet the above specifications. The City purchases its stops at: *Interstate Traffic Control, San Carlos (650) 591-2300*

7. Install the Parklet, the safe-hit posts, and the parking stops in one week or less. Inform us at [sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org) 48 hours before installation is complete.



upon receipt of permit:  
notify us of installation date

2 weeks before install:  
have signs fabricated

72 hours before install:  
place no-parking signs

PLAN

PREPARE

INSTALL

3 weeks before install:  
purchase safe-hit posts

10 days before install:  
schedule DPW site inspection

48 hours before completion:  
notify us by email

SAN FRANCISCO  
PLANNING DEPARTMENT

Questions regarding  
Parklet Permits and  
the Pavement to Parks  
program should be  
directed to:

**ANDRES POWER** Project Manager  
Planning Department  
415.558.6384  
[sfpavementtoparks@sfgov.org](mailto:sfpavementtoparks@sfgov.org)

**ATTACHMENT B**

Documents establishing San Francisco's Pavement to Parks Program

*San Francisco's Parklet Maintenance  
Application*



San Francisco City and County  
**Department of Public Health**  
**Environmental Health Section**  
Consumer Protection

Gavin Newsom, Mayor  
Mitchell H. Katz, Director of Health  
Rajiv Bhatia, M.D., M.P.H.  
Director of Environmental Health

**Parklet Installation Information & Maintenance**

Location: 896 Sutter Street San Francisco CA 94109      Dates: \_\_\_ XXX \_\_\_

Contact Person: Joseph Y Dabit      Phone Number: 415-577-1901

Parking Meter Numbers: N.A. Block does not contain parking meters

---

Restaurant Name: Cup-A-Joe Coffee House      Phone Number: (415) 563-7745

Address: 896 Sutter Street San Francisco CA 94109

Contact Person: Joseph Y Dabit      Phone Number: 415-577-1901

---

Licensed Pest Control Company \*: \_\_\_ XXX-XXX-XXX \_\_\_      Phone Number: \_\_\_ XXX-XXX-XXX \_\_\_

Frequency of service: \_\_\_ XXX \_\_\_      (e.g. every 2 weeks, monthly)

\* NOTE: pest control reports are to be kept available at the restaurant or by parklet permit holder at all times

Pest control methods used beneath parklet (circle):      bait (mandatory)

Only use if able to access underside frequently:      snap traps      glue boards

---

Platform Cleaning Scheduled (top):      daily      weekly

Enclosed Bench Cleaning Scheduled:      daily      weekly

Platform Cleaning Scheduled (below):      weekly      every 2 weeks      monthly

Dates: \_\_\_ XXX \_\_\_

***Retain for your records & return a copy to the address below along with platform diagram indicating access and service panels before platform installation, renewal, or change in permit holder information.***

District Inspector

Phone (415) 252-3800

1390 Market Street, Suite 210, San Francisco, CA 94102

fax (415) 252-3842

## ATTACHMENT C

### Photographs of Parklets in San Francisco, Chicago, Oakland, and Long Beach

#### SAN FRANCISCO PARKLETS

---

##### Noe Valley Parklet



Photo by Andres Power



Photo by Andres Power

##### Columbus Ave Parklet



PROPOSED

Photo by Rebar Group



CONSTRUCTED

Photo by SF Bicycle Coalition

##### Powell Street Parklet



## 22<sup>nd</sup> Street Parklet



PROPOSED

Photo by Rebar Group



CONSTRUCTED

Photo by Rebar Group



CONSTRUCTED  
Group

Photo by Rebar

## Divisadero Street



Photo by sfbiker



Photo by sfbiker

## OAKLAND PARKLETS

---



## CHICAGO PARKLETS

---



Andersonville Parklet



Lakeview Parklet

## LONG BEACH PARKLETS



**ATTACHMENT D**  
Site plans for Pilot Parklet Projects in Los Angeles

*615 Spring Street Parklet*

*639 Spring Street Parklet*

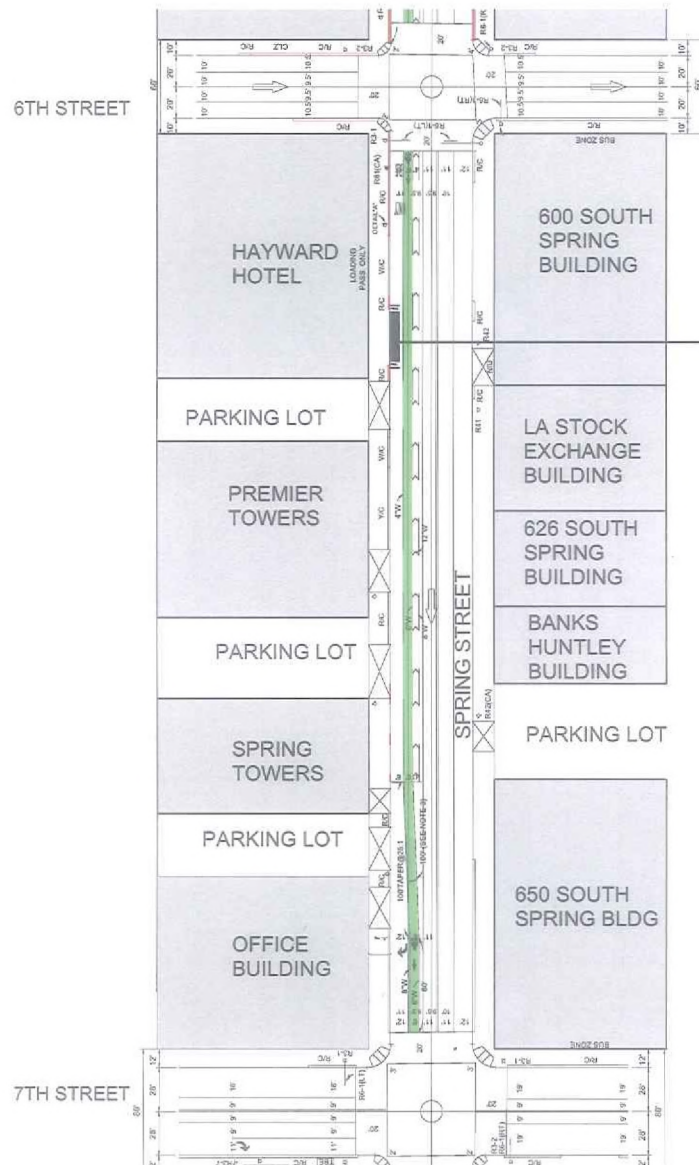
*Huntington Drive & Rosemead Avenue Plaza Parklet*

*York Avenue & 51<sup>st</sup> Street Porch Parklet*

**ATTACHMENT D**  
Site plans for Pilot Parklet Projects in Los Angeles

***615 Spring Street Parklet***

# SPRING STREET PILOT PARKLET



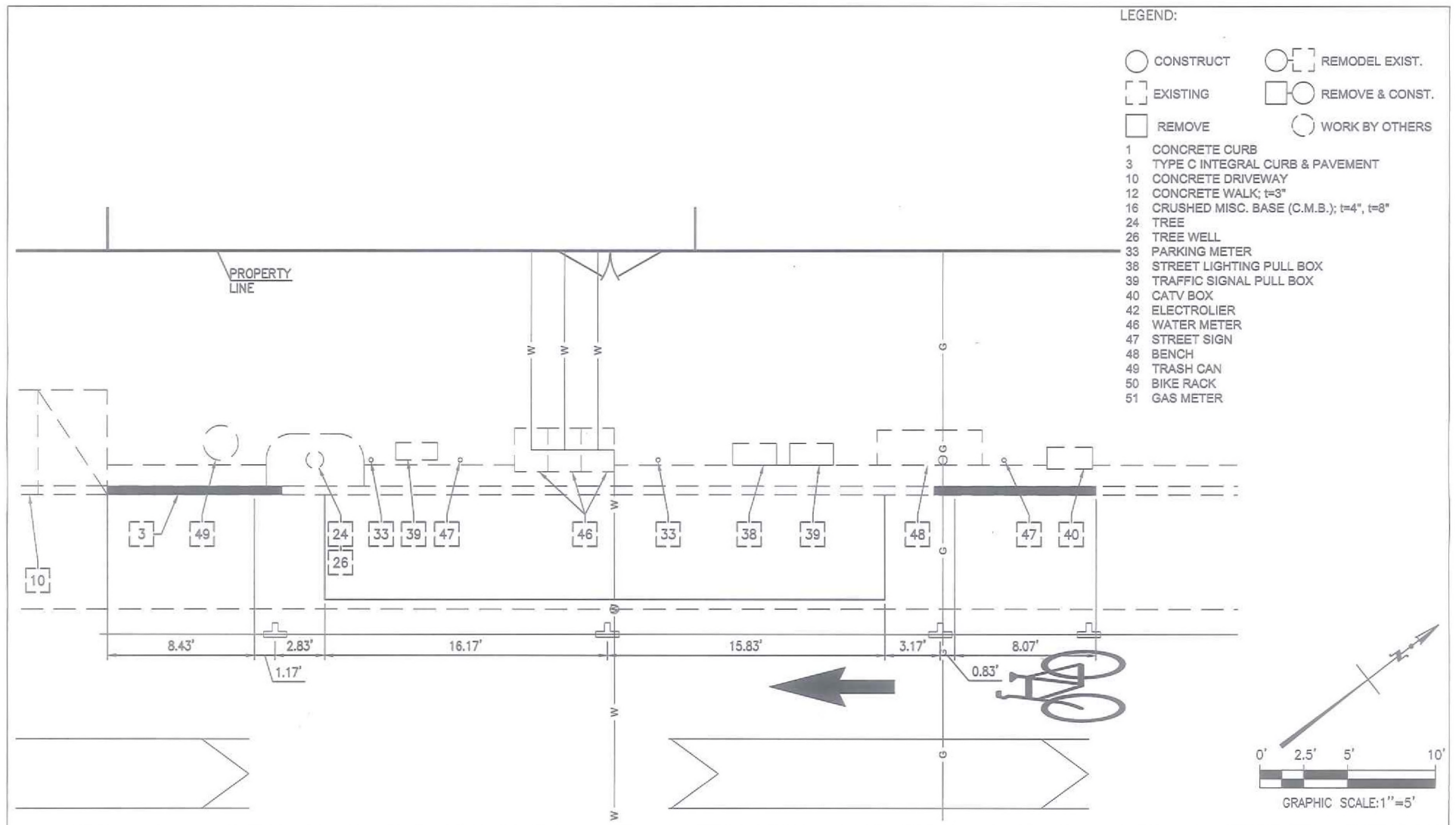
**S I T E**  
615 Spring Street  
Los Angeles, CA  
90014



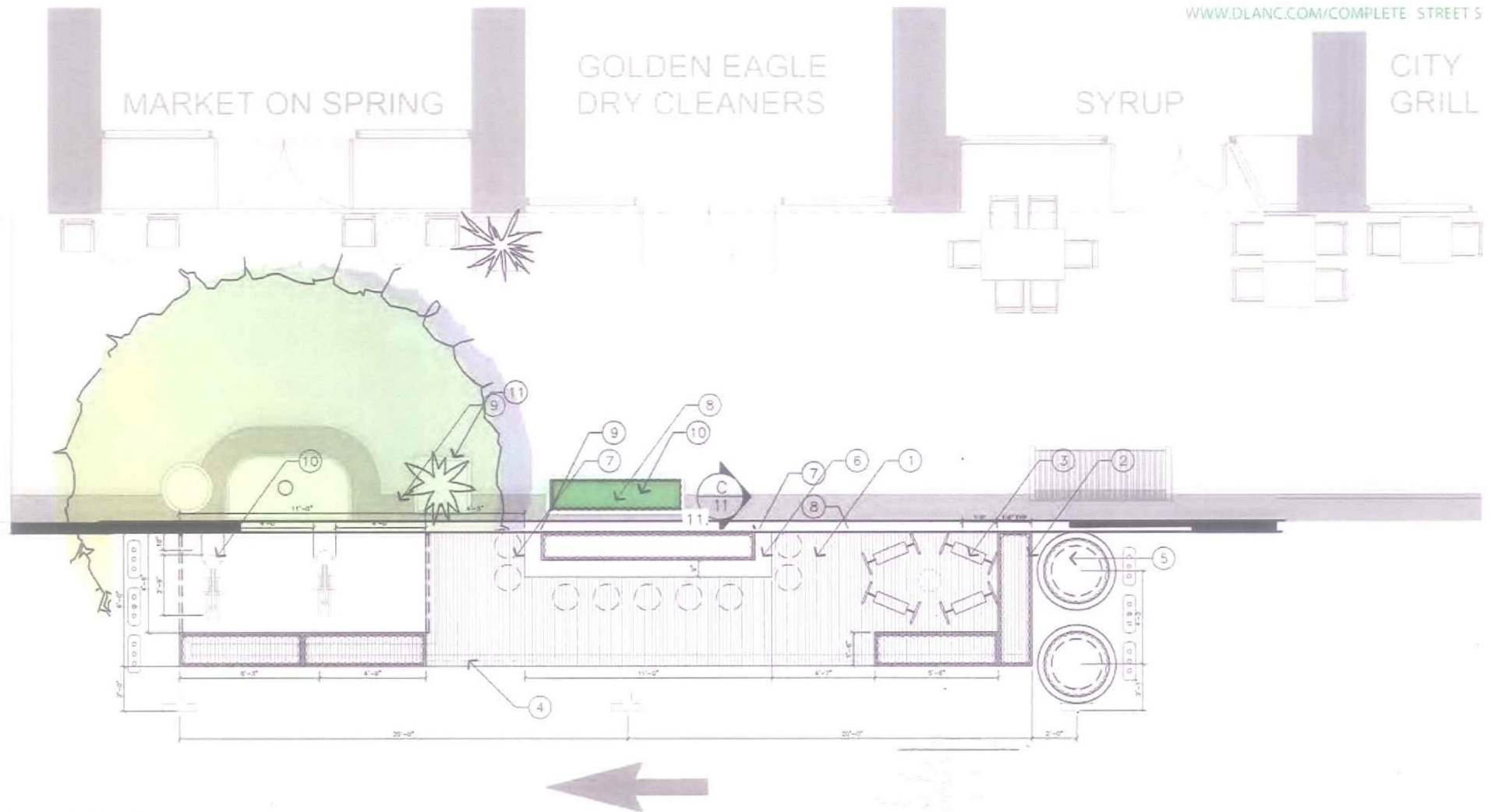
LOCATION

SPRING STREET  
PILOT PARKLET

2012 AUG 16  
1 / 7



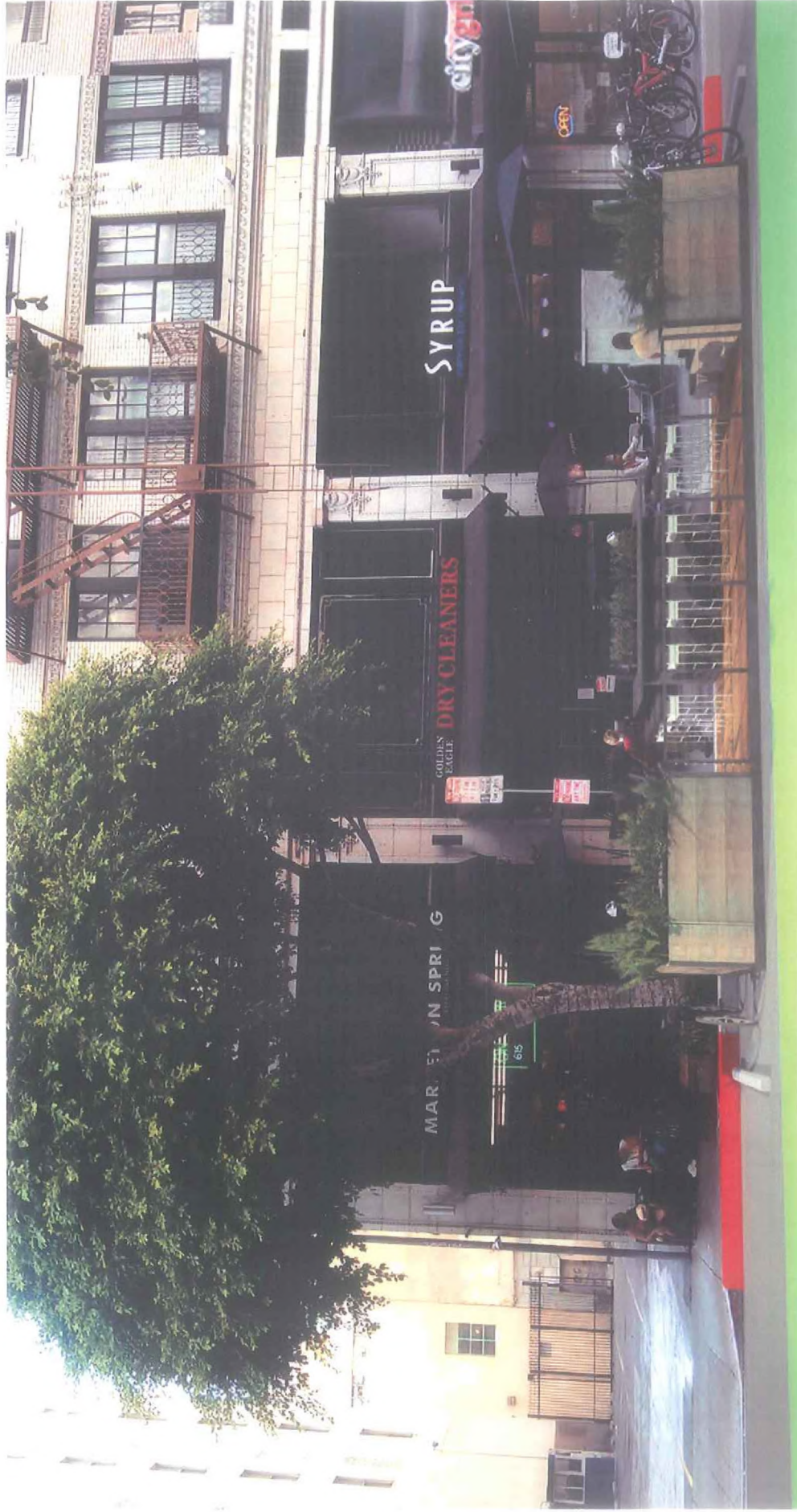
# AS-BUILT SITE PLAN



# LEGEND

1. WOOD DECKING ON PEDESTALS.
2. 30" HT. PLANTER BOX, TYP.
3. SURFACE-MOUNTED CUSTOM DOUBLE-SIDED SWING CHAIR.
4. GRAPHICS AND SAFETY REFLECTIVE MESH PERIMETER TREATMENT.
5. 30" HT. VEHICULAR RATED 'ARMERIA' TERRACAST PLANTERS ANCHORED TO PAVEMENT.
6. RAISED DINING COUNTERTOP.
7. MOVEABLE SEATING.
8. EXISTING AT GRADE PLANTER.
9. EXISTING POT TO BE RELOCATED.
10. EXERCISE BIKE ANCHORED TO ROADBED.

## PROPOSED PLAN



## PROPOSED FRONT ELEVATION



VIEW FROM NORTH



VIEW FROM SOUTH

## PLANT SCHEDULE

WWW.DLANC.COM/COMPLETESTREETS

### SHRUB SCHEDULE:

SYMBOL	ABBR.	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
-	AEO KIW	AEONIUM 'KIWI'	KIWI AEONIUM	1 GAL	PER PLAN
-	CRA CAM	CRASSULA CAPITELLA 'CAMPFIRE'	CAMPFIRE CRASSULA	1 GAL	PER PLAN
-	ECH NOD	ECHEVERIA NODULOSA	PAINTED ECHEVERIA	1 GAL	PER PLAN
-	FES GLA	FESTUCA GLAUCA	BLUE FESCUE	1 GAL	PER PLAN
-	HEL LIM	HELICHRYSUM PETIOLARE 'LIMELIGHT'	LIMELIGHT LICORICE PLANT	1 GAL	PER PLAN
-	HEL SEM	HELIOTOTRICHON SEMPERVIRENS	BLUE OAT GRASS	1 GAL	PER PLAN
-	KAL PUM	KALANCHOE PUMILA	FLOWER DUST PLANT	1 GAL	PER PLAN
-	PHO RAI	PHORMIUM 'RAINBOW MAIDEN'	RAINBOW MAIDEN NEW ZEALAND FLAX	1 GAL	PER PLAN
-	SEN MAN	SENECIO TALINOIDES VAR. MANDRALISCAE	BLUE FINGER	1 GAL	PER PLAN



AEONIUM 'KIWI'  
KIWI AEONIUM



HELIOTOTRICHON  
SEMPERVIRENS  
BLUE OAT GRASS



CRASSULA CAPITELLA  
'CAMPFIRE'  
CAMPFIRE CRASSULA



ECHEVERIA NODULOSA  
PAINTED ECHEVERIA



FESTUCA GLAUCA  
BLUE FESCUE



HELICHRYSUM PETIOLARE  
'LIMELIGHT'  
LIMELIGHT LICORICE PLANT



KALANCHOE PUMILA  
FLOWER DUST PLANT



PHORMIUM 'RAINBOW MAIDEN'  
RAINBOW MAIDEN  
NEW ZEALAND FLAX



SENECIO TALINOIDES VAR.  
MANDRALISCAE  
BLUE FINGER

## PLANT SCHEDULE

SPRING STREET 2012 AUG 16  
PILOT PARKLET 7 / 10



PARKLET VOLUME CALCULATION

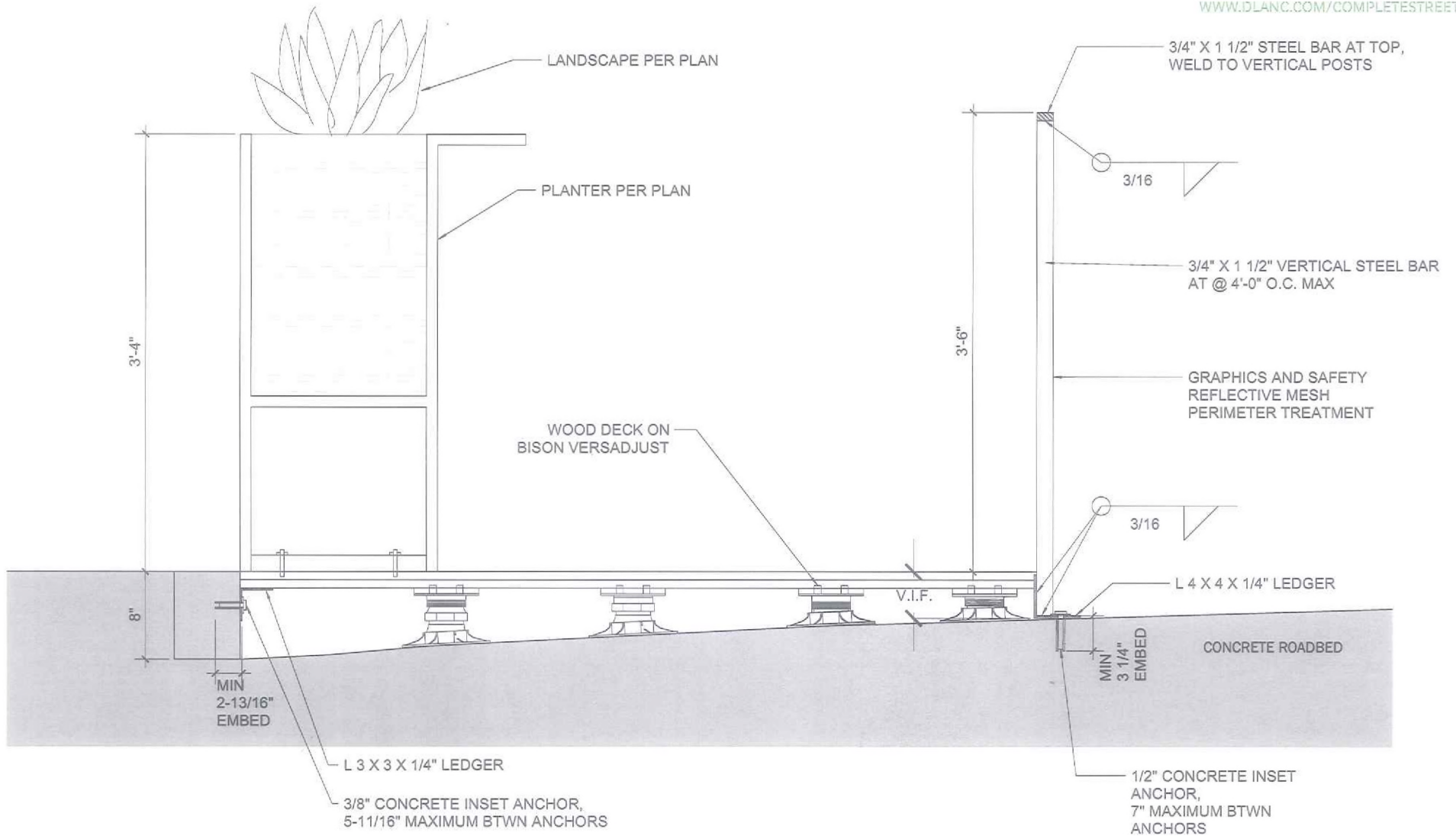
BASE VOLUME =  $105.4 \text{ ft}^3$   
- DECK & SUPPORTS =  $17.3 \text{ ft}^3$

---

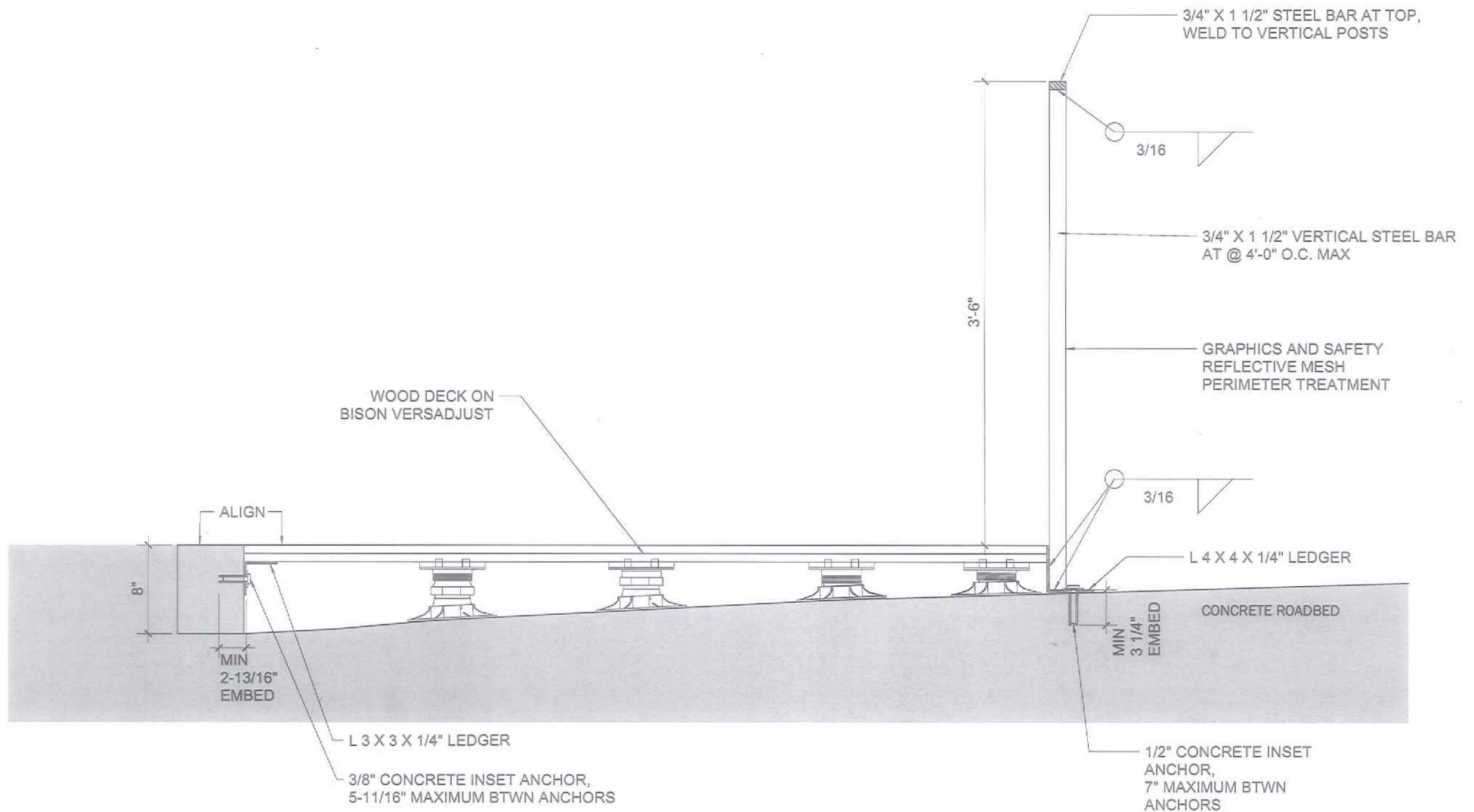
OPEN VOLUME =  $88.1 \text{ ft}^3$

**83% UNOBSTRUCTED**

PEDESTAL AND DECK AXONOMETRIC



## TYPICAL SECTION



## TYPICAL SECTION

**ATTACHMENT D**  
Site plans for Pilot Parklet Projects in Los Angeles

**639 Spring Street Parklet**

## Cover Letter for Final Parklet Drawings - 639 Spring St.

The following revisions/calculations are provided based on prior feedback:

- North end planters will be filled with sand and coarse gravel. The exact weight will be recorded with the A-permit request.
- Wheel stops are incorporated at both ends of the parklet.
- Each Bison pedestal is rated at 1,500 lbs per pedestal. The pedestal area is approximately 246 sq. ft, using 111 pedestals. Therefore, the overall sub-structure has a load bearing capacity of 676 lbs/sq in.
- Sign will be incorporated into the site to warn users they are using the site at their own risk (see figure 1).
- See figure 2 for friction co-efficients from the paver manufacturer (see figure 2).
- Planters will be affixed to the pavement using a concrete inset drop-in anchor.
- Graphic panel next to the exercise area will be able to swing out for handicapped access.
- A mesh panel was added to the northern end of the parklet as requested.
- The roof drain is noted on the as-built plan and does not drain into the parklet site.

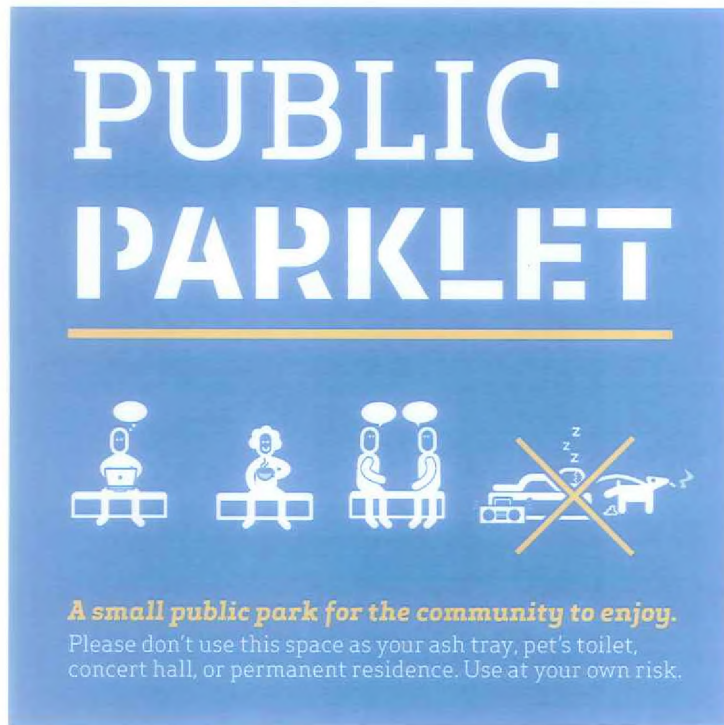


Figure 1



## SMITH-EMERY LABORATORIES

(An Independent Commercial Testing Laboratory)

781 E. Washington Boulevard - 2nd Floor - Los Angeles, California 90021 ♦ (213) 745-5333 ♦ Fax (213) 749-7232

Project Job No.: 38535-1  
Laboratory No.: T-09-145

August 27, 2009

Client: KEVIN COURTNEY  
STEP STONE, INC  
17025 S. MAIN STREET  
GARDENA, CA 90248

Subject: 12" x 12" x 2-1/2" Thick California Architectural Paver Light Sand Blaster Concrete  
Specification: ASTM C 1028-07  
Source: Picked up by Smith-Emery Laboratories Representative on August 12, 2009

### STATIC COEFFICIENT OF FRICTION (ASTM C 1028-07)

A block of wood with a 3" x 3" x 1/8" section of standard neolite side liner attached, was placed on the surface to be tested, on top of this assembly, a 50 pound (22kg) weight was placed. Using dynamometer, the force in pounds required to cause the test assembly to slip parallel to the test surface was measured. Four measurements were taken on each of three test surfaces, each measurement perpendicular to the previous one. The twelve measurements were averaged to obtain the coefficient of friction for each test condition.

#### A. As Received:

Test Condition	Tile No.	N	E	S	W	Average	Individual Static Coefficient of Friction (fc)	S.C.O.F. After Neolite Correction Factor
Dry Neolite	1	46	47	47	45	45.42	(0.89)	0.84
	2	46	44	47	45			
	3	45	45	44	44			
Wet Neolite	1	44	44	44	44	43.67	(0.85)	0.83
	2	44	43	43	43			
	3	44	44	43	44			

#### B. After Cleaning with Hilliards Renovator. (ASTM C 1028 Standard Cleaner)

Dry Neolite	1	44	46	46	45	45.17	(0.88)	0.83
	2	45	44	45	45			
	3	45	46	45	46			
Wet Neolite	1	49	49	50	48	49.00	(0.96)	0.94
	2	49	48	49	50			
	3	48	50	49	49			

Respectfully Submitted,  
SMITH-EMERY LABORATORIES

P. John Latiolat  
Registered Civil Engineer No. C60312  
Registration Expires: 06-30-10



Specification: Department of Justice ADA Title III Regulation 28 CFR Part 36, Section A4.5.1; Recommends minimum of 0.60 SCOF for horizontal surfaces and 0.80 SCOF on ramps.

- ☒ Materials Tested Comply With Specifications.  
☒ Horizontal; ☒ Ramps or Incline  
☐ Materials Tested Did Not Comply With Specifications.  
☐ No Established Criteria for Acceptable Limits.  
☐ For Information Only.

CC: STEP STONE, INC./SMITH-EMERY LABORATORIES

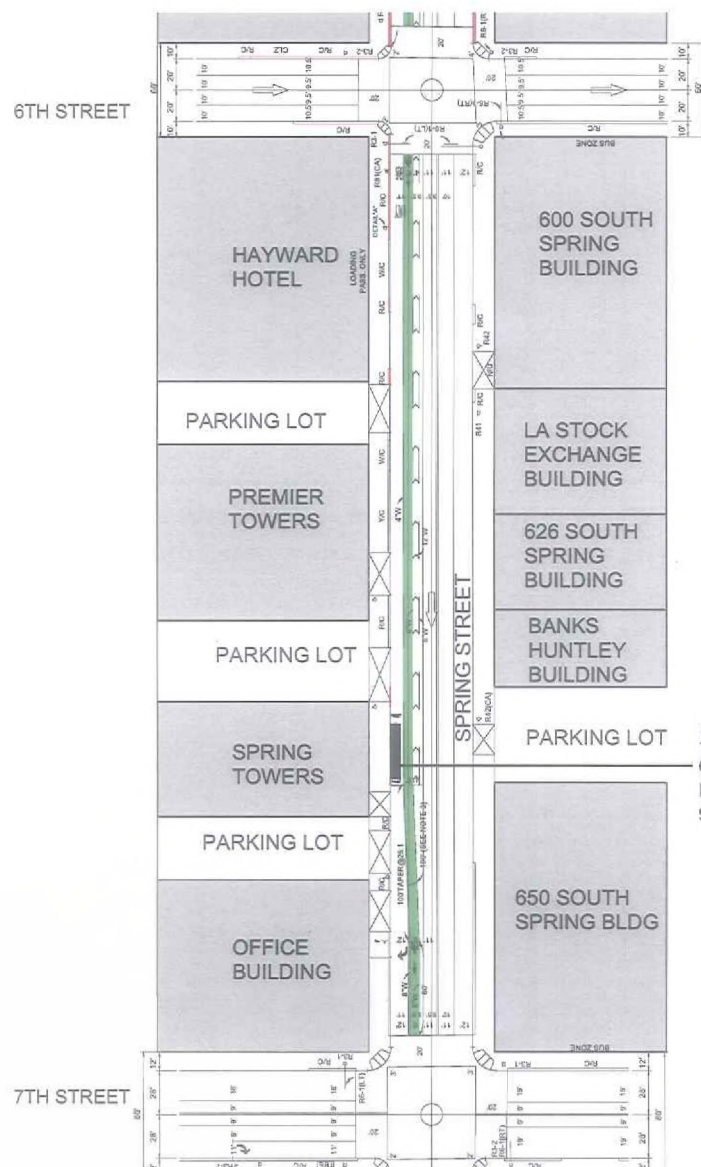
G:\PHYLAB\2009\38535 Step Stone, Inc Job T-09-145 SCOF.xls

Page 2 of 5

ALL REPORTS ARE SUBMITTED AS THE CONFIDENTIAL PROPERTY OF CLIENTS. AUTHORIZATION FOR THE REUSE OF ANY REPORT, DATA, OR RESULTS IS LIMITED TO THE PROJECT AND ANY REUSE OF INFORMATION REGARDING THEM IS RESERVED TO PHILLIPS & LATIOLAT. APPROVAL AS A VIRTUAL PROFESSIONAL ENGINEER. THE PHILLIPS & LATIOLAT

Figure 2

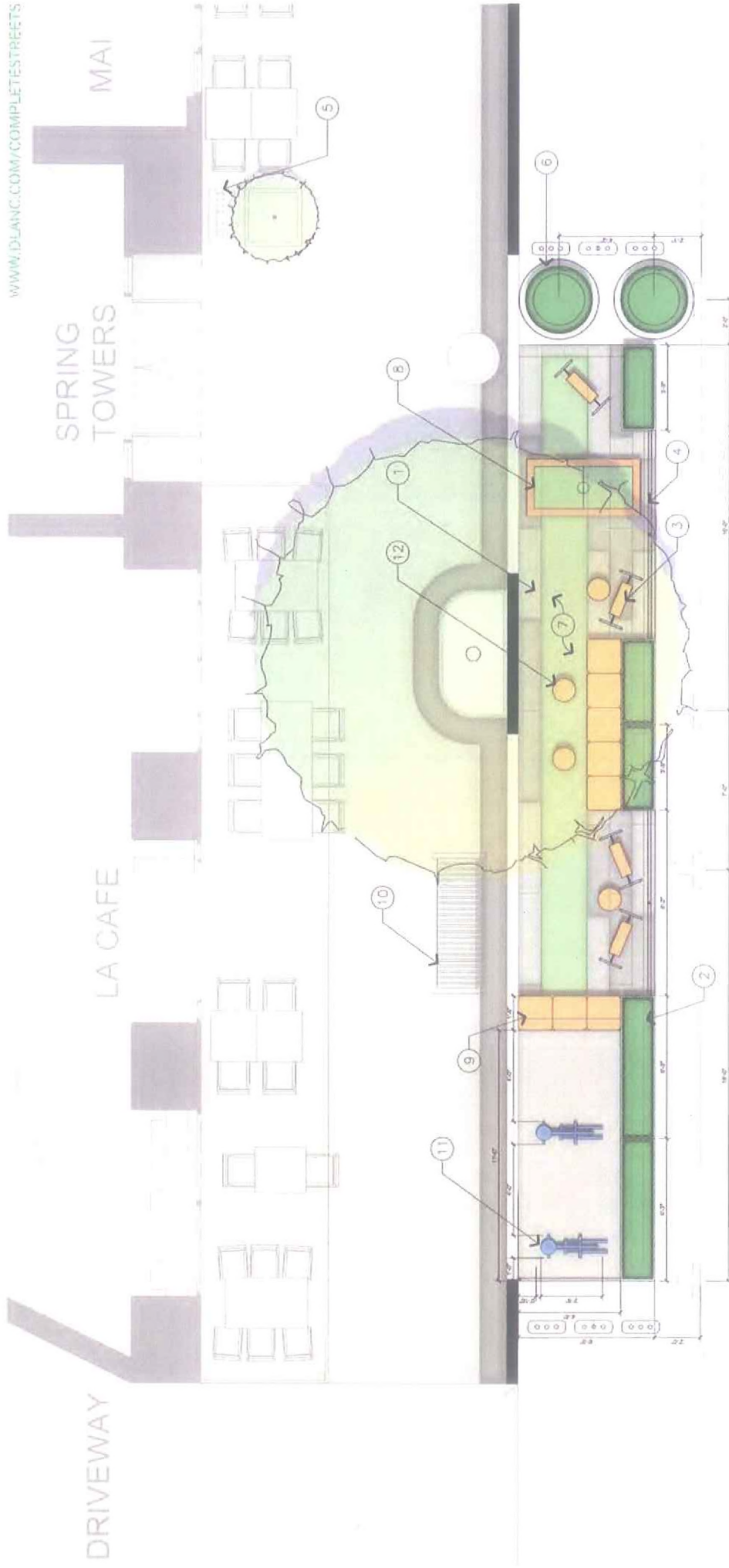
# SPRING STREET PILOT PARKLET



LOCATION







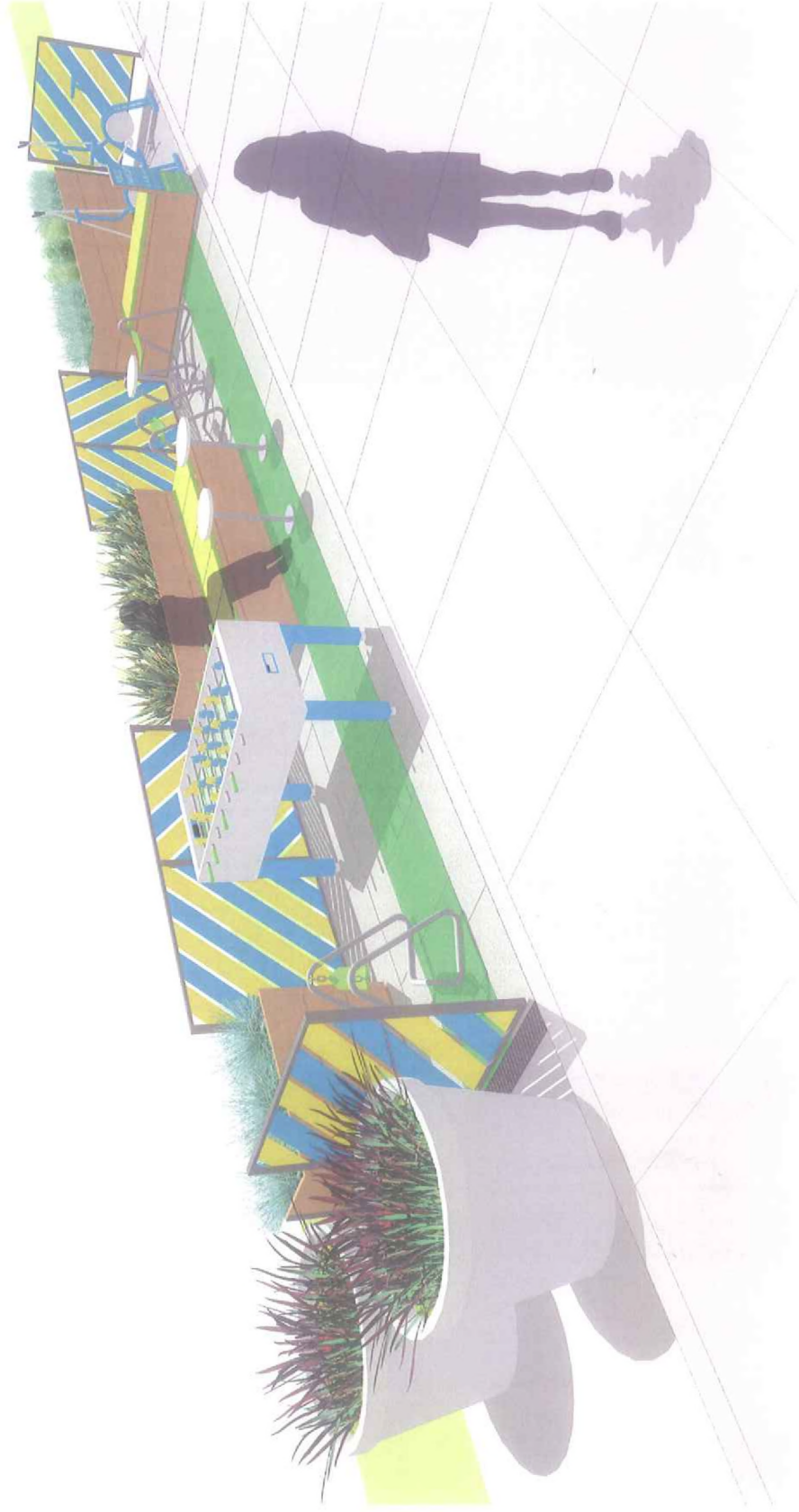
# LEGEND

1. PRE-CAST CONCRETE PAVERS ON PEDESTALS
2. 24" HT. PLANTER BOX, TYP.
3. CUSTOM DOUBLE-SIDED SWING CHAIR
4. GRAPHICS AND SAFETY REFLECTIVE MESH PERIMETER TREATMENT
5. EXISTING STAND PIPE
6. 30" HT. VEHICULAR RATED ARMERIA TERRACAST PLANTERS
7. SYNTHETIC TURF ADHERED TO PRE-CAST CONCRETE PAVERS
8. OUTDOOR FOOTBALL TABLE
9. BUILT-IN BENCH SEATING
10. EXISTING BENCH, TO BE RELOCATED
11. EXERCISE BIKE ANCHORED TO ROAD BED
12. MOVEABLE TABLETOP FURNISHINGS

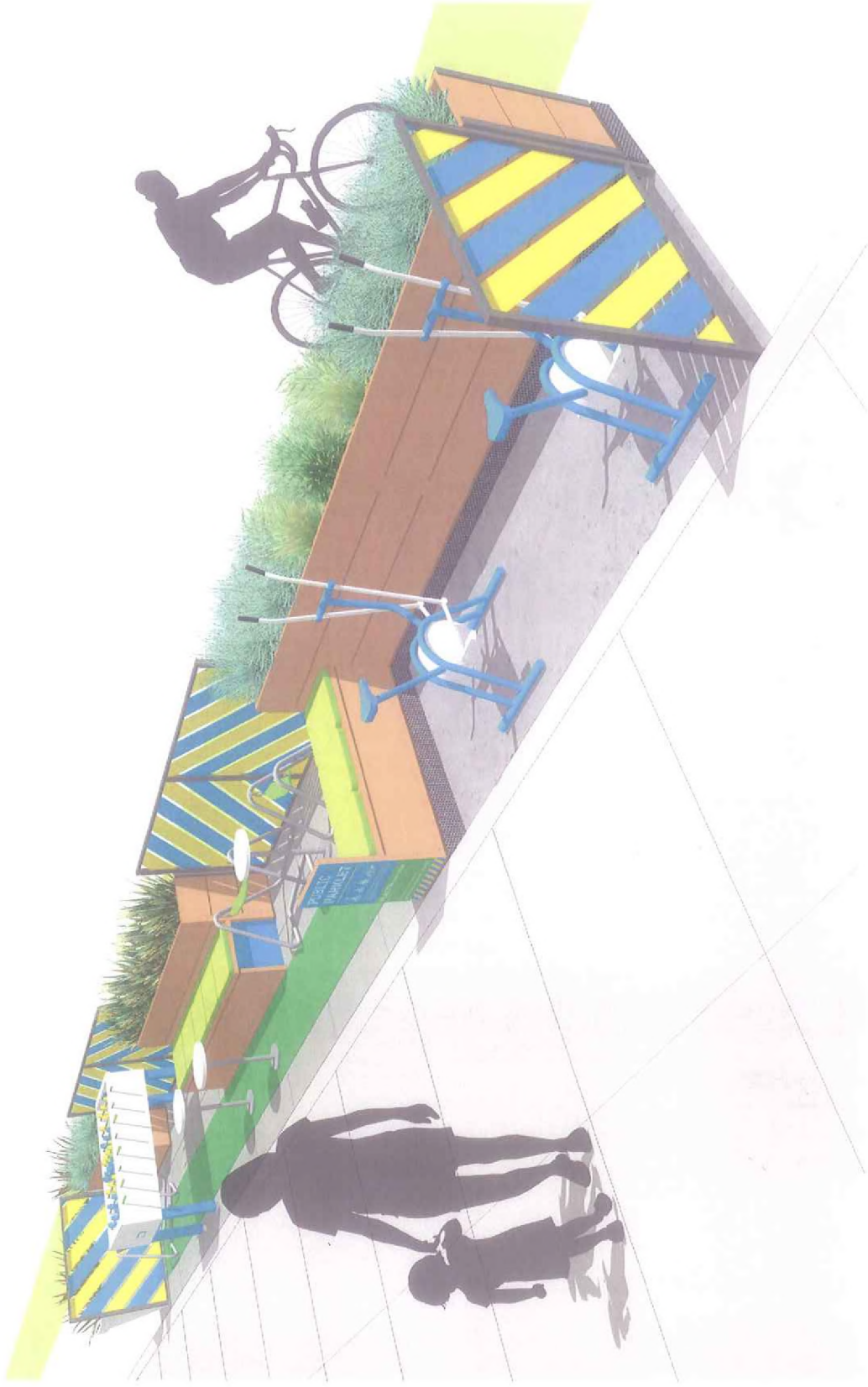
## PROPOSED PLAN



## PROPOSED FRONT ELEVATION



VIEW FROM NORTH



VIEW FROM SOUTH

# PLANT SCHEDULE

WWW.DLANC.COM/COMPLETESTREETS

## SHRUB SCHEDULE:

SYMBOL	ABBR.	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
-	AEO KIW	AEONIUM 'KIWI'	KIWI AEONIUM	1 GAL	PER PLAN
-	CRA CAM	CRASSULA CAPITELLA 'CAMPFIRE'	CAMPFIRE CRASSULA	1 GAL	PER PLAN
-	ECH NOD	ECHEVERIA NODULOSA	PAINTED ECHEVERIA	1 GAL	PER PLAN
-	FES GLA	FESTUCA GLAUCA	BLUE FESCUE	1 GAL	PER PLAN
-	HEL LIM	HELICHRYSUM PETIOLARE 'LIMELIGHT'	LIMELIGHT LICORICE PLANT	1 GAL	PER PLAN
-	HEL SEM	HELICTOTRICHON SEMPERVIRENS	BLUE OAT GRASS	1 GAL	PER PLAN
-	KAL PUM	KALANCHOE PUMILA	FLOWER DUST PLANT	1 GAL	PER PLAN
-	PHO RAI	PHORMIUM 'RAINBOW MAIDEN'	RAINBOW MAIDEN NEW ZEALAND FLAX	1 GAL	PER PLAN
-	SEN MAN	SENECIO TALINOIDES VAR. MANDRALISCAE	BLUE FINGER	1 GAL	PER PLAN



AEONIUM 'KIWI'  
KIWI AEONIUM



HELOCTOTRICHON  
SEMPERVIRENS  
BLUE OAT GRASS



CRASSULA CAPITELLA  
'CAMPFIRE'  
CAMPFIRE CRASSULA



ECHEVERIA NODULOSA  
PAINTED ECHEVERIA



FESTUCA GLAUCA  
BLUE FESCUE



HELICHRYSUM PETIOLARE  
'LIMELIGHT'  
LIMELIGHT LICORICE PLANT



KALANCHOE PUMILA  
FLOWER DUST PLANT



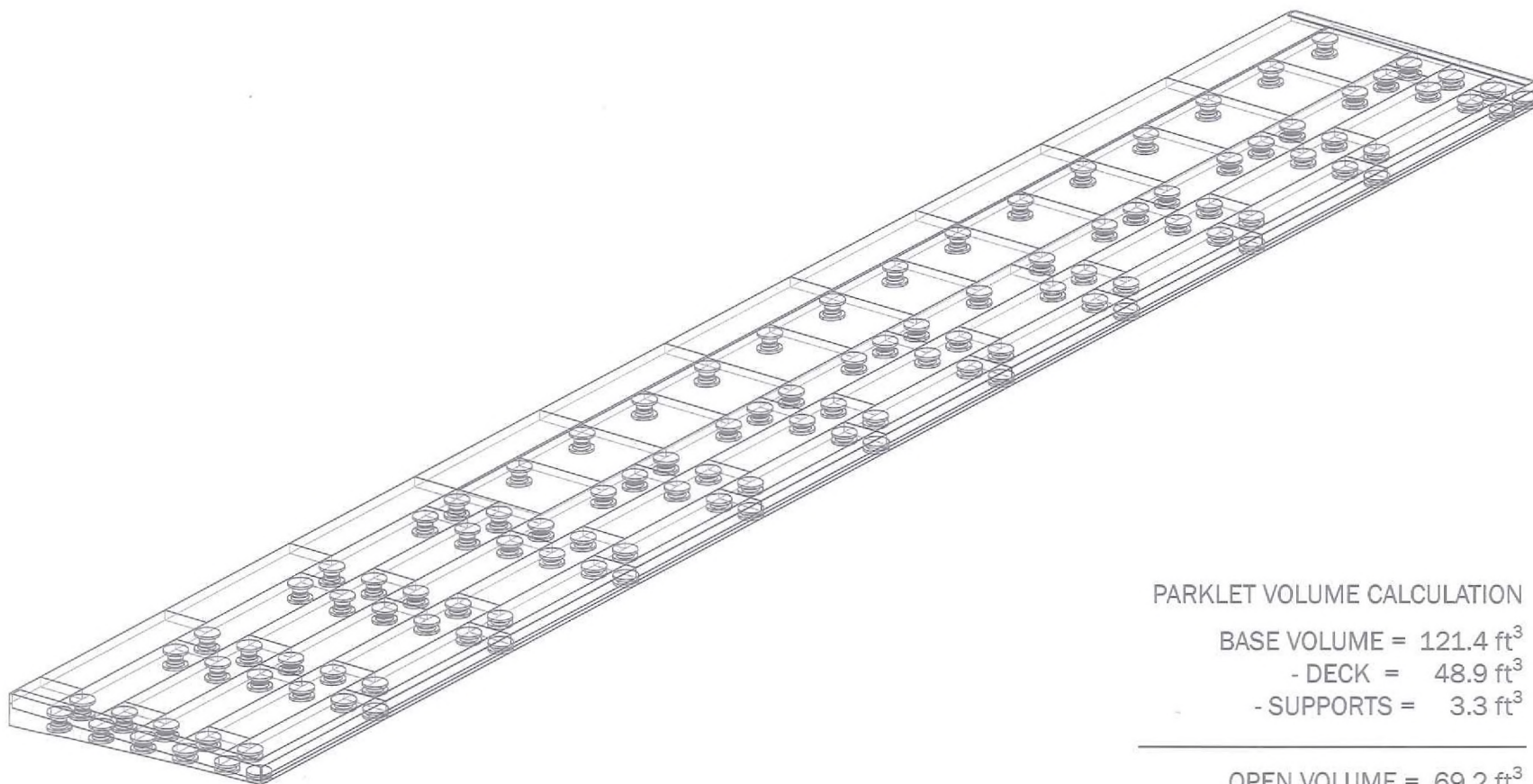
PHORMIUM 'RAINBOW MAIDEN'  
RAINBOW MAIDEN  
NEW ZEALAND FLAX



SENECIO TALINOIDES VAR.  
MANDRALISCAE  
BLUE FINGER

# PLANT SCHEDULE

SPRING STREET  
PILOT PARKLET  
2012 AUG 16  
7 / 10



PARKLET VOLUME CALCULATION

BASE VOLUME =  $121.4 \text{ ft}^3$

- DECK =  $48.9 \text{ ft}^3$

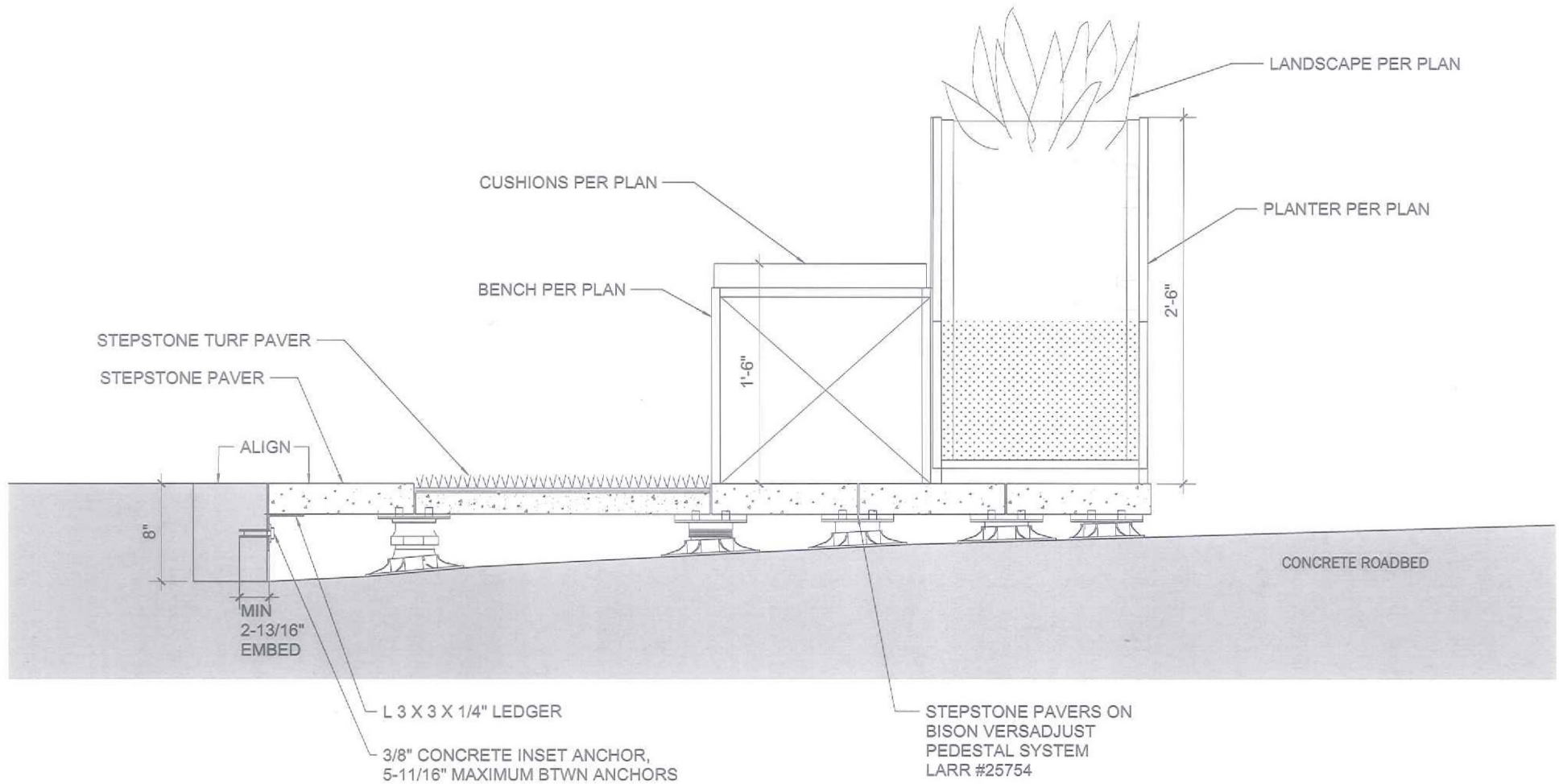
- SUPPORTS =  $3.3 \text{ ft}^3$

---

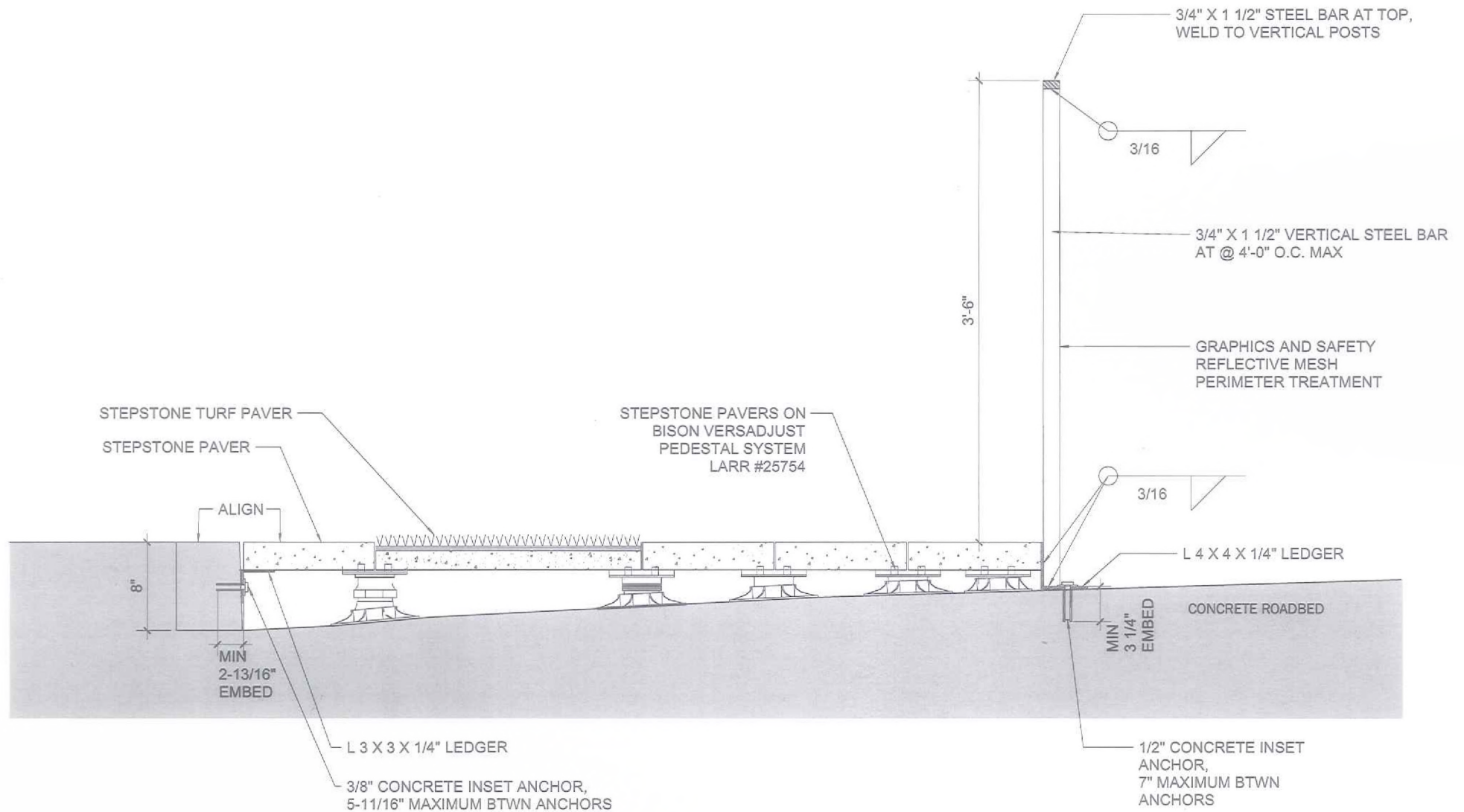
OPEN VOLUME =  $69.2 \text{ ft}^3$

**57% UNOBSTRUCTED**

PEDESTAL AND DECK AXONOMETRIC



## TYPICAL SECTION



## TYPICAL SECTION

**ATTACHMENT D**

Site plans for Pilot Parklet Projects in Los Angeles

*Huntington Drive & Rosemead Avenue Plaza Parklet*



Site viewed from east looking west on Huntington Drive.

## "Our Downtown" El Sereno Street Plaza

Living Streets LA

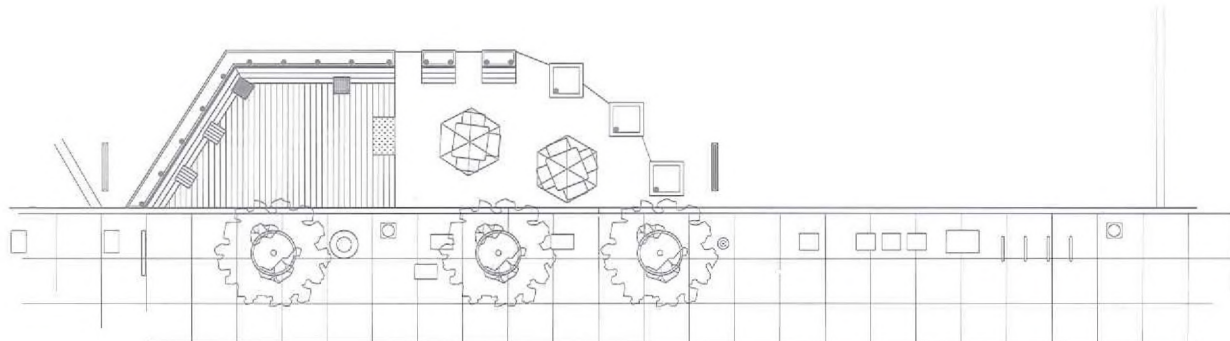
**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

### General Notes

1. Project will be constructed by the Los Angeles Conservation Corps as part of a Design/Build collaboration with Shared Spaces Landscape Architecture.
2. No aspect of this project will alter the sidewalk, curb or roadbed with the exception of the installation of approved anchors (see below).
3. All elements to be anchored using appropriate steel brackets and appropriate fasteners: inset concrete anchors on sidewalk pavement and hex and carriage bolts on deck and planters. Size, length and gauge for each application to be determined by contractor unless specified in plan.
4. Project will display parklet/street porch pilot program signage, including warning that use is at visitors own risk.

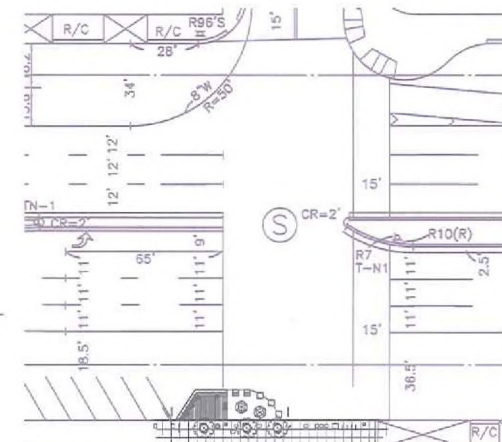
### Index

- L0.1 Overview and Notes
- L0.2 Layout Plan
- L0.3 Planting Plan
- L0.4 Deck and Bench Plan
- L0.5 Deck and Bench Details, Page 1
- L0.6 Deck and Bench Details, Page 2
- L0.7 Furnishing



Site Plan

SCALE OF FEET  
3/8" = 1'-0"



Site Plan located on DOT striping plan for orientation. Note striping plan does not appear to be accurately to scale. Refer to layout plan for actual placement of project in relation to intersection line and crosswalk.

Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Overview and Index

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.1**

1 of 7 Sheets

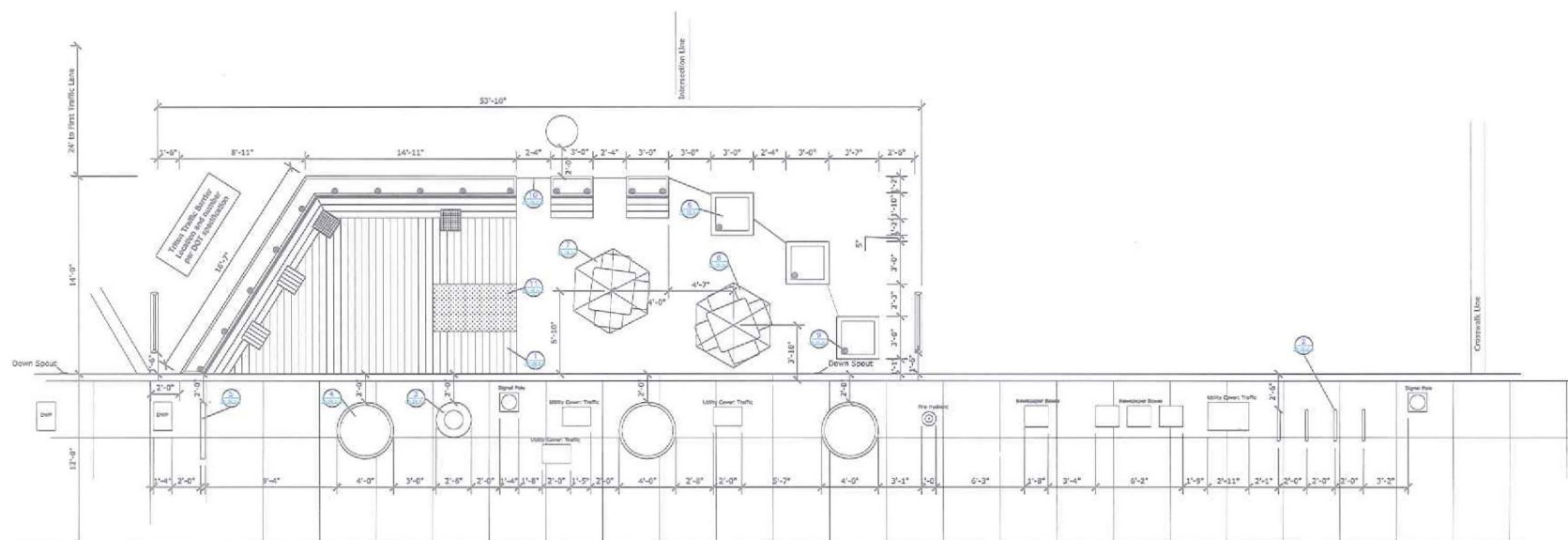
Notes:

"Our Downtown"  
El Sereno  
Street Plaza

Living Streets LA

SHARED SPACES

318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218



## Layout Plan



**Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps**

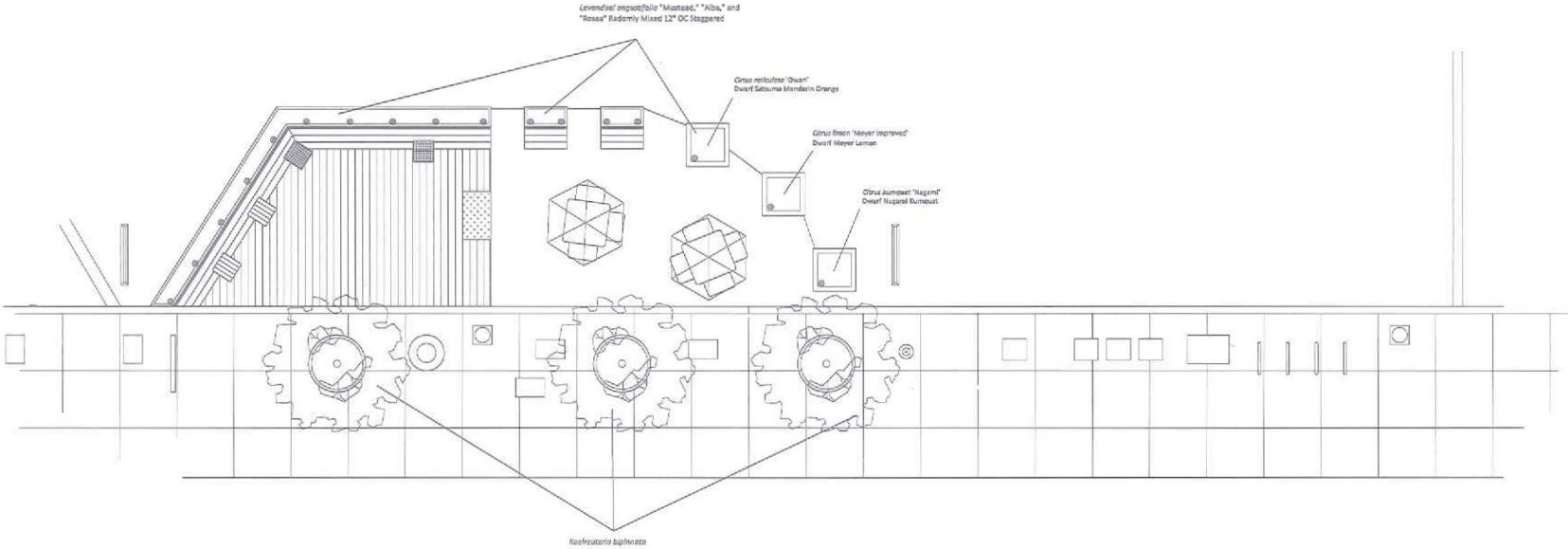
SHEET TITLE:  
Layout Plan

DATE:  
August 13, 2012

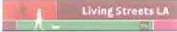
SHEET NUMBER:  
**L0.2**  
2 of 7 Sheets

Notes:  
1. All plant materials to be approved by Shared Spaces before installation

Name	Common Name	Number	Size	Notes
Koeleruteria bipinnata	Chinese Flame Tree	3	24" Box	Single leader to 8' or higher
Citrus reticulata 'Owan'	'Owan' Satsuma Mandarin Orange	1	15 gal	Flying Dragon Dwarf, max height in container 3'
Citrus limon 'Meyer improved'	Meyer Lemon	1	15 gal	Flying Dragon Dwarf, max height in container 3'
Citrus kumquat 'Nagami'	Nagami Kumquat	1	15 gal	Flying Dragon Dwarf, max height in container 3'
Lavandula angustifolia 'Alba'	English Lavender 'Alba'	33	6"	
Lavandula angustifolia 'Rosea'	English Lavender 'Rosea'	33	6"	
Lavandula angustifolia 'Mustead'	English Lavender 'Mustead'	33	6"	



"Our Downtown"  
El Sereno  
Street Plaza



**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

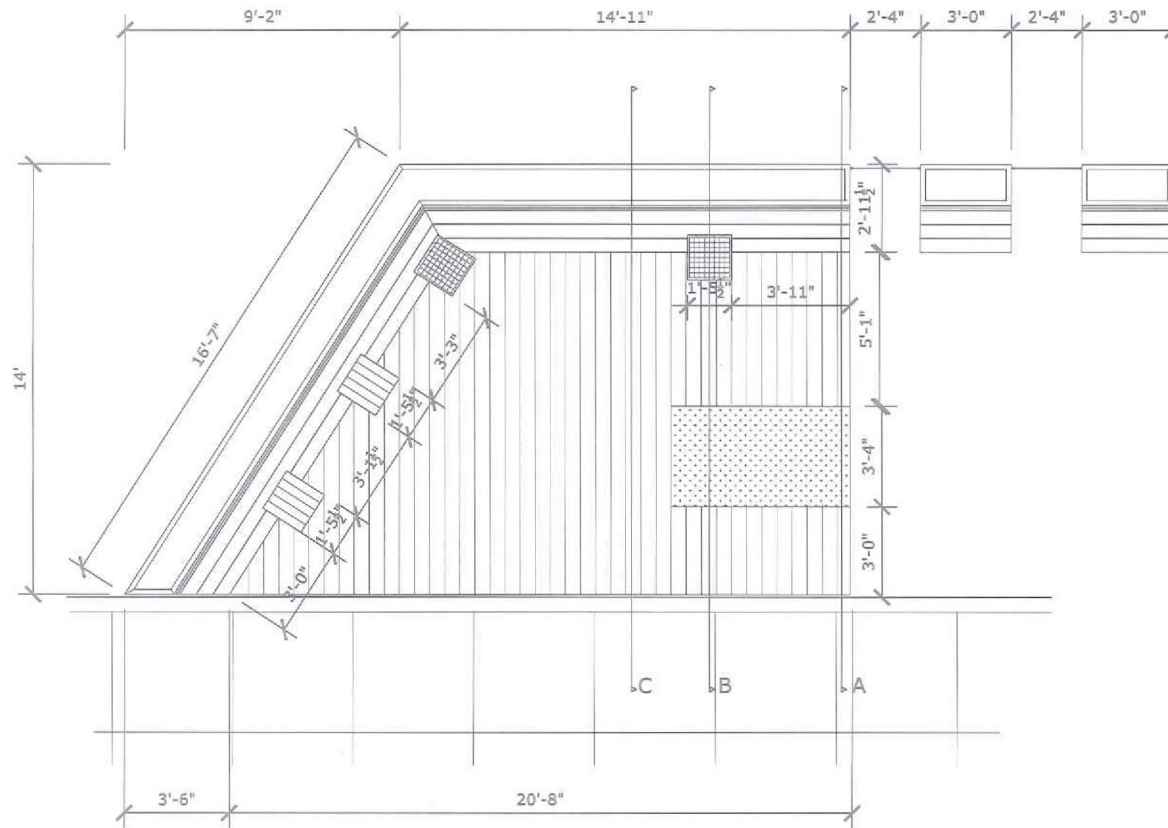
SHEET TITLE:  
Planting Plan

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.3**  
3 of 7 Sheets

# Notes:

1. All lumber FSC Certified Redwood
2. All assembly to be with galvanized or stainless deck screws, lag screws and bolts and carriage bolts of gauge and length determined by contractor unless otherwise noted.



Deck Plan



"Our Downtown"  
El Sereno  
Street Plaza

Living Streets LA

**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

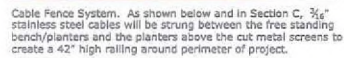
Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Deck and Bench Plan

DATE:  
August 13, 2011

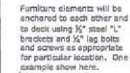
SHEET NUMBER:  
**L0.4**  
4 of 7 Sheets

1. All lumber FSC certified redwood.
2. All assembly to be with galvanized or stainless deck screws, lag screws and bolts and carriage bolts of gauge and length determined by contractor unless otherwise noted.
3. Deck joists to be trimmed on site to compensate for crown of street, leaving deck with no more than 1 percent grade toward sidewalk.



10  
10.2 | 10.6

## Perforated Metal Fence Screens



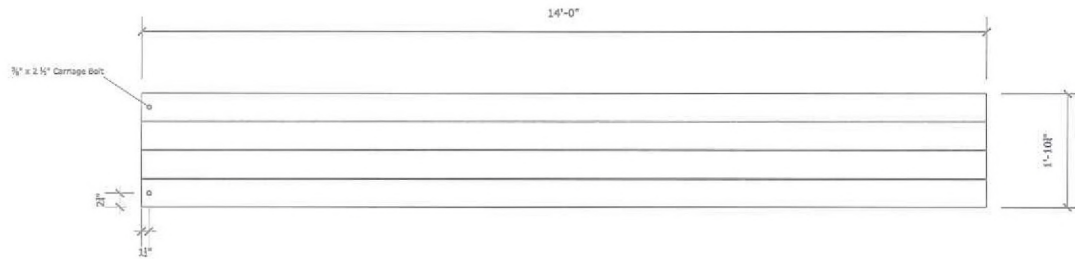
Deck Elevation and Sections A, B and C

5 of 7 Sheets

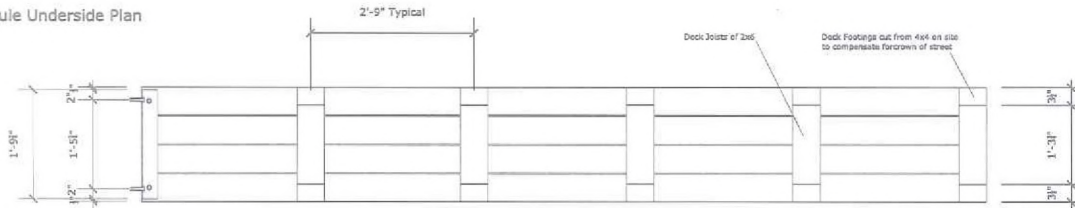
## Notes:

1. All lumber FSC certified redwood.
2. All assembly to be with galvanized or stainless deck screws, lag screws and bolts and carriage bolts of gauge and length determined by contractor unless otherwise noted.
3. Deck joists to be trimmed on site to compensate for crown of street, leaving deck with no more than 2 percent grade toward sidewalk.

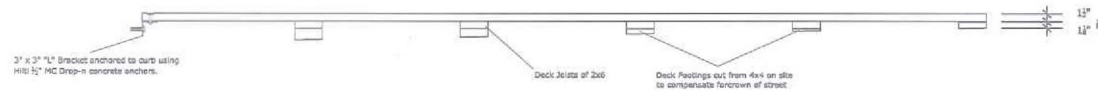
### Module Plan



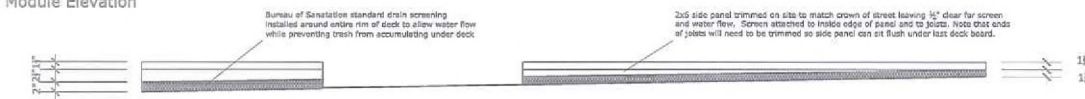
### Module Underside Plan



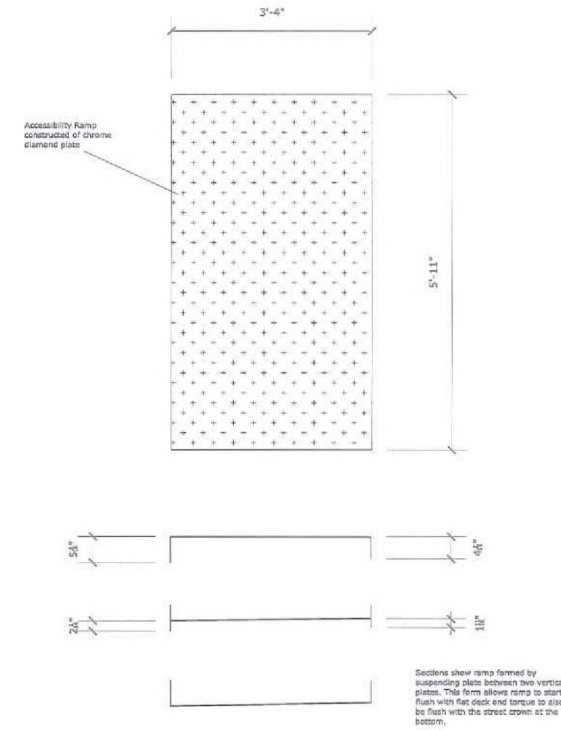
### Module Elevation



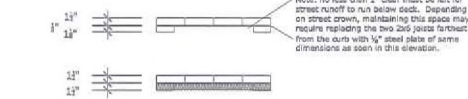
### End Module Elevation



### Deck Module Details



### Accessibility Ramp Details



### Module Elevations Viewed From Street

(Shown with and without screen for clarity.)



"Our Downtown"  
El Sereno  
Street Plaza

Living Streets LA

**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Deck and Bench Details  
Page 2 of 2

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.6**  
6 of 7 Sheets



LA DOT Inverted U Bike Rack

2  
1/2/0/0



Highland Products 30 gallon welded metal trash receptacle, Product 166-1248, Yellow.

3  
1/2/0/0



Quikrete concrete planter, Greenfield plant flower pot style QR-GF5040P, 48" x 40", Terracota. Note image shows narrower, taller planter from same line.

4  
1/2/0/0



Global Industries Outdoor Message Center, W1248635WW, Weathered Wood. Windows constructed of break-resistant acrylic and easily replaceable.

5  
1/2/0/0



Quikrete concrete planter, California style, QS-CAL3636P, 36"x 36"x33", Terracota.

6  
1/2/0/0



Highland Product, powder coated, expanded metal table with Capri seats, yellow.

7  
1/2/0/0



Park n' Pool Contoured Aluminum Vane Umbrella, 28UM-00002, Yellow.

8  
1/2/0/0



Brickmann Emperor solar garden light, 822-0420-0-4

9  
1/2/0/0

"Our Downtown"  
El Sereno  
Street Plaza

Living Streets LA

**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Furnishing

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.7**  
7 of 7 Sheets

**ATTACHMENT D**

Site plans for Pilot Parklet Projects in Los Angeles

*York Avenue & 51<sup>st</sup> Street Porch Parklet*



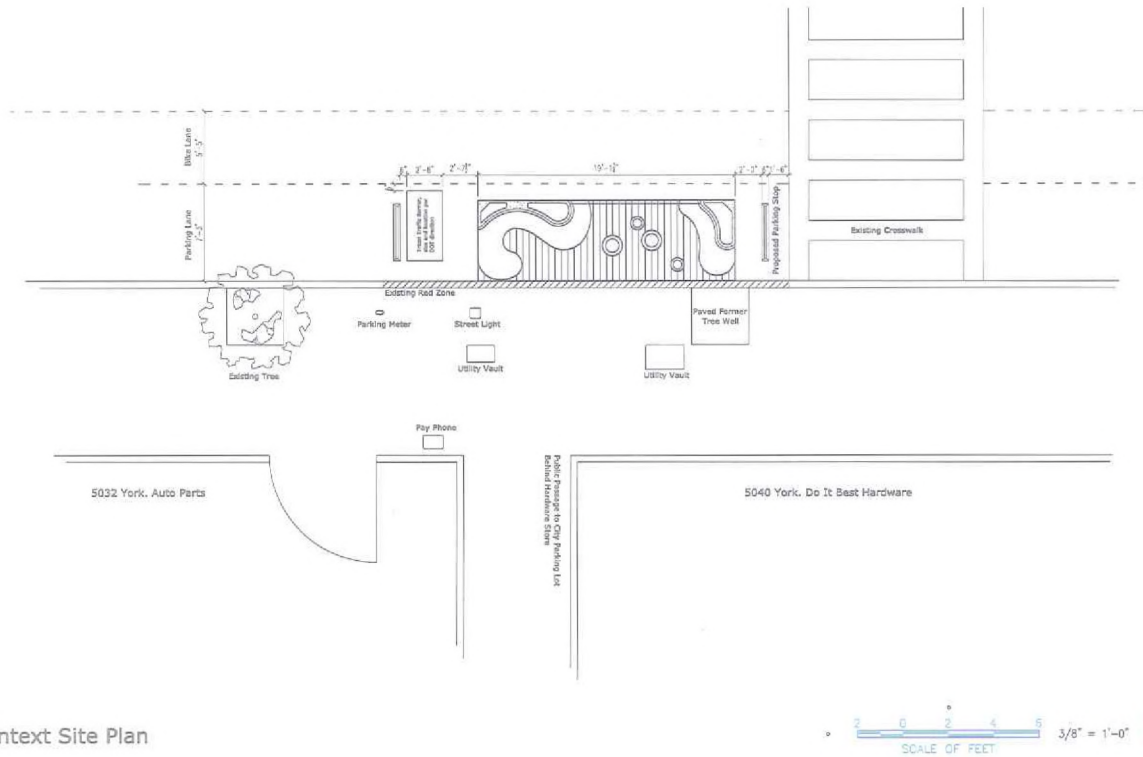
Site viewed looking east down York Blvd.  
For orientation, note crosswalk seen on east/right side of site plan below.

### General Notes

1. Project will be constructed by the Los Angeles Conservation Corps as part of a Design/Build collaboration with Shared Spaces Landscape Architecture.
2. No aspect of this project will alter the sidewalk, curb or roadbed with the exception of the installation of approved anchors.
3. Project will display parklet/street porch pilot program signage, including warning that use is at visitors own risk.

### Index

- L0.1 Overview and Notes
- L0.2 Layout Plan
- L0.3 Sections
- L0.4 Details



Context Site Plan

York Blvd.  
Ave 50-51  
Street Porch

Living Streets LA  
**SHARED SPACES**  
318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

Second Revision  
Pilot Project Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

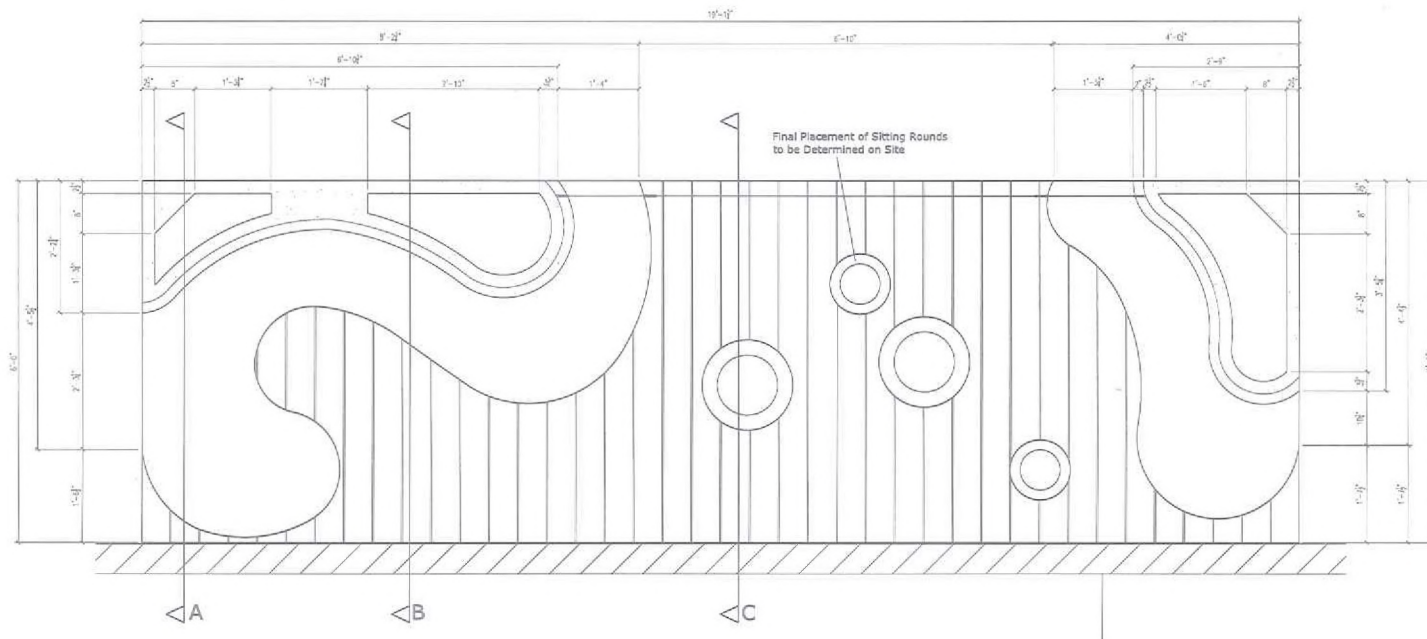
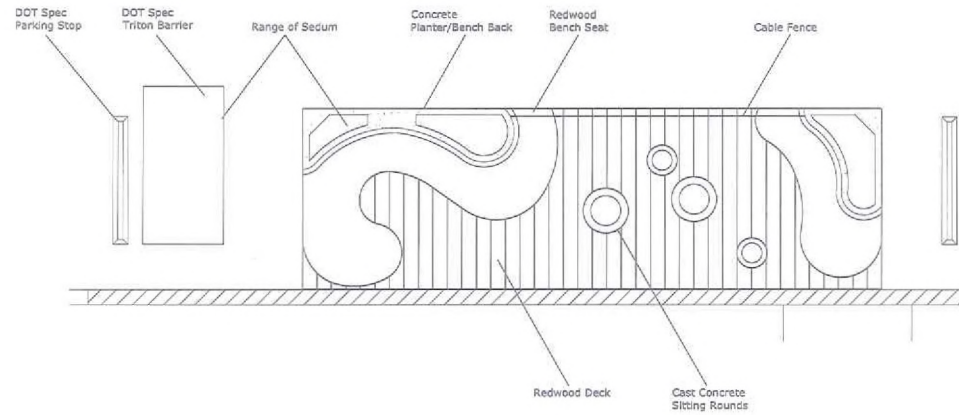
SHEET TITLE:  
Overview and Index

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.1**  
1 of 4 Sheets

# Notes:

1. Parking Stops to be anchored with DOT spec hardware.
2. Lumber bases for benches and concrete forms for bench back/planters should be created following full scale 1:1 print of layout plan to match curvilinear forms. Dimensions of curvilinear forms are to be used to cross-check critical measurements, but matching these measurements is not sufficient. Only tracing forms from 1:1 plan will fulfill intent.



Layout Plan



York Blvd.  
Ave 50-51  
Street Porch



**SHARED SPACES**

318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-405-5426 415-409-2218

Second Revision  
Pilot Project Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Layout Plan

DATE:  
August 13, 2012

SHEET NUMBER:  
**L0.2**  
2 of 4 Sheets

## Notes:

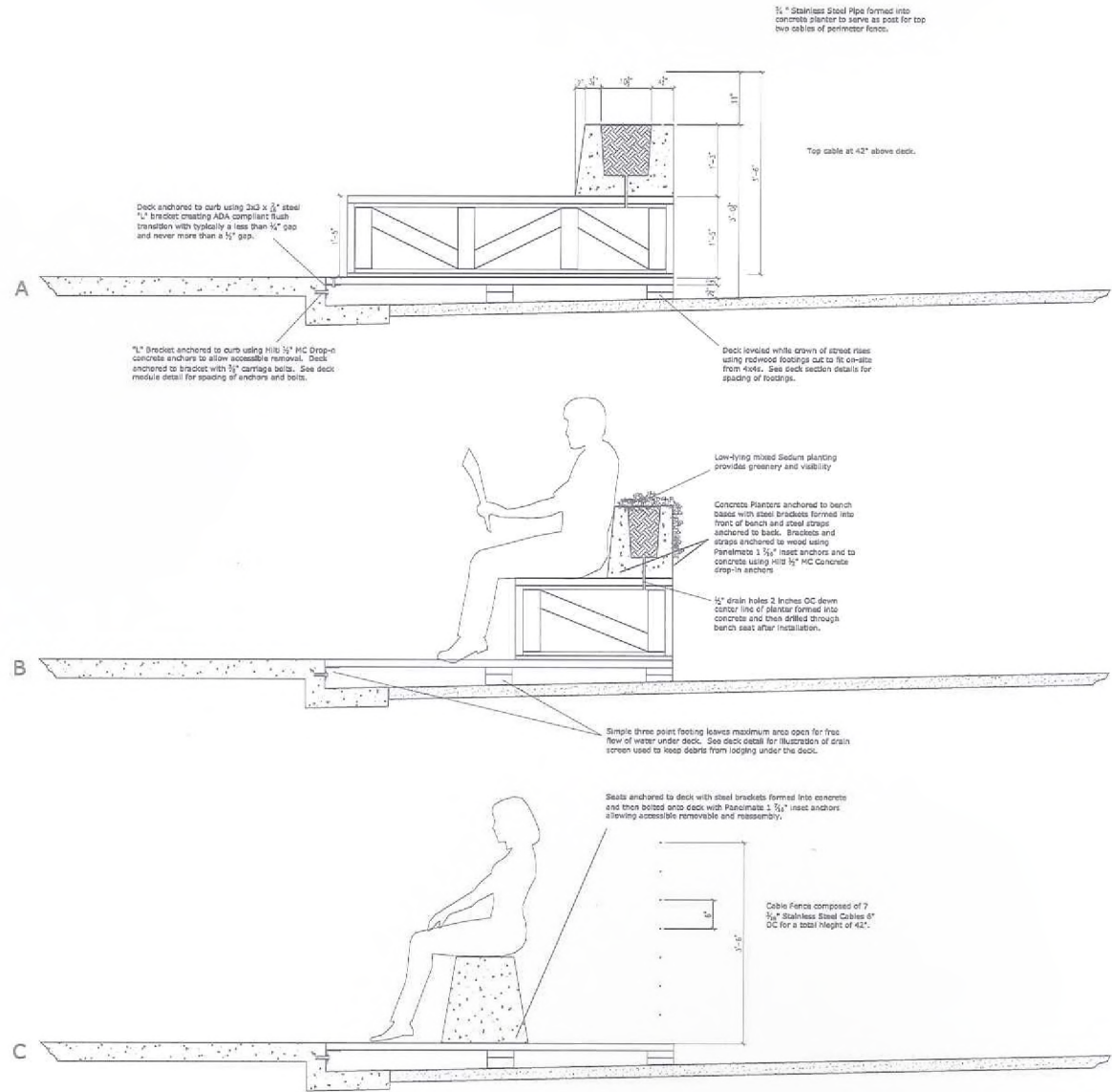
1. All lumber FSC certified redwood.
2. All assembly to be with galvanized or stainless deck screws, lag screws and bolts, carriage bolts, and brackets of gauge and length determined by contractor unless otherwise noted.
3. Bench bases to be anchored to deck using  $\frac{1}{2}$ " steel brackets and  $\frac{1}{2}$ " lag bolts or screws as appropriate for location.
4. Concrete seats and planters to be anchored to benches and decks using  $\frac{1}{2}$ " steel brackets formed into concrete and anchored into concrete as shown in details below.
5. Deck footings to be trimmed on site to compensate for crown of street, leaving deck flat.



Example Mosaic Bench. Concrete planter/bench backs and concrete seat rounds will include mosaic tile in manner similar to this image. Contractor to provide surface and bed needed for mosaic in direction of project mosaic artist.



Cable Fence System.  $\frac{1}{4}$ " stainless steel cables will be strung between the two planter benches at 5' intervals as indicated on the layout plan and in section C.



## Sections



York Blvd.  
Ave 50-51  
Street Porch

Living Streets LA

**SHARED SPACES**

318 S. Alvarado, LA, CA 90057  
1960 Bush, SF, CA 94115  
213-400-5426 415-409-2218

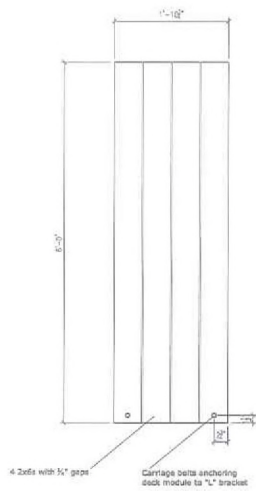
Second Revision  
Pilot Program Set  
Design/Build  
Construction Documents  
for Implementation by  
Los Angeles  
Conservation Corps

SHEET TITLE:  
Sections

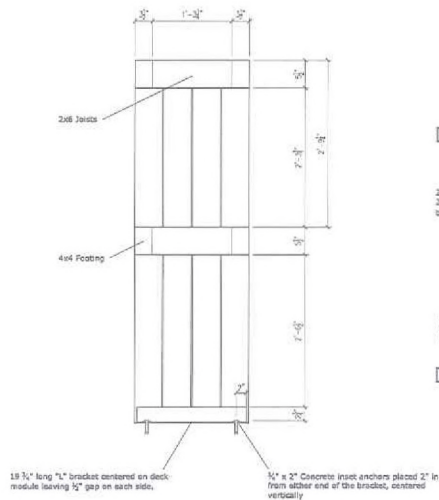
DATE:  
August 13, 2011

SHEET NUMBER:  
**L0.3**

3 of 4 Sheets



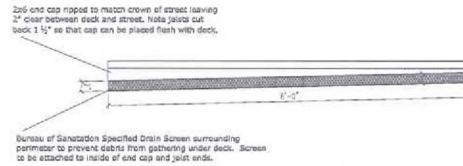
Deck Module Plan



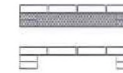
Deck Module Underside



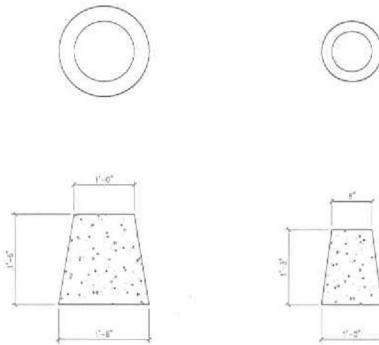
Deck Module Typical Section



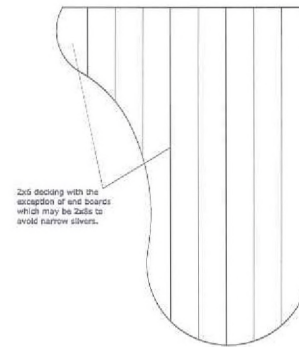
Deck Module End Elevation



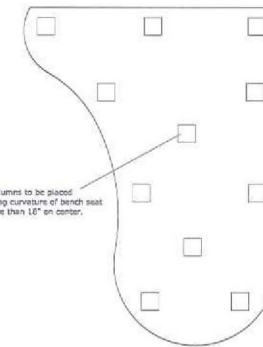
Deck Module Elevation from Street  
(Shown with and without screen for clarity.)



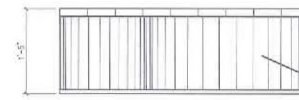
Cast Concrete Seat Rounds



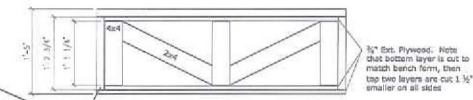
Bench Seat Decking Plan



Bench Seat Column Plan



Bench Elevation



Bench Interior Structure